

**MACEDONIA PLANNING COMMISSION
PUBLIC HEARING
JUNE 12, 2023**

TIME: 5:15 p.m.

CALL TO ORDER:

ROLL CALL: Mr. Cox, Ms. Meske, Mr. Westbrooks, Mr. Roberts, and Mr. Wallenhorst.

- 1) Mr. Volas is proposing a Conditional Zoning Certificate for Cleveland Clinic to operate a medical facility in a B-2 Community Shopping District located at 2055 Alexandria Way.

---CLOSE PUBLIC HEARING---

**MACEDONIA PLANNING COMMISSION
MEETING AGENDA
JUNE 12, 2023**

Location: Macedonia City Hall
Council Chambers
9691 Valley View road
Macedonia, Ohio 44056

Time: 5:30 P.M.

Call to Order

Roll Call:

- Mr. Westbrooks
- Mr. Cox
- Mr. Wallenhorst
- Mr. Roberts
- Ms. Meske

Approval of the May 15, 2023 Public Hearing minutes.
Approval of the May 15, 2023 Planning Commission minutes.

Agenda Items:

- 1) Mr. Volas is proposing a Conditional Zoning Certificate approval to operate a medical facility in a B-2 Community Shopping District located at 2055 Alexandria Way.

Peter volas
9500 Euclid Ave.
Cleveland, OH 4419521
216-312-4255
volasp@ccf.org

- 2) Mr. Greenburg, site consultant, is proposing site modifications for The Rack retail store located at 500 E. Aurora Rd.

Eric Greenberg
Site Consultant
3601 Green Rd, Suite 302
Beachwood, OH 44122
216-650-4170
egreenberg@siteconsult.com

- 3) Mr. Cospers with Cupkovic Architecture LLC is proposing store front modifications for Sephora located at 500 E. Aurora Rd.

Craig Cospers
Cupkovic Architecture
6060 Rockside Woods Blvd. North Suite 100
Independence, OH 44131
216-524-8400
ccospers@cuparc.com

- 4) Mr. Panter is proposing site plan approval for the drive-thru lanes at Chick-Fil-A located at 8213 Golden Link Blvd.

Jake Panter
Approved Representative Kimley-horn
3875 Embassy Pkwy, Suite 280
Akron, OH 44333
630-487-3432
Jake.panter@kimley-horn.com

- 5) Review of the maximum building height language.

Ordinance 44-2017 (1167.02(g)) – B-1 Convenience Business Districts
Ordinance 67-2017 (1167.03(g)) – B-2 Community Shopping g Districts
Ordinance 68-2017 (1167.04(g)) – B-3 General Business Districts
Ordinance 69-2017 (1167.05(g)) -- B-4 Highway Business Districts
Ordinance 70-2017 (1167.06(g)) -- B-O Business Office Districts

Miscellaneous:

Adjournment

Tabled Items:

Continued Items:

07/18/2022 Proposed electric vehicle charging stations located at 8100 Macedonia Commons Blvd.

03/20/2023 Proposed preliminary plan approval for a new hotel located at 20 Girl Scout Way.

04/17/2023 Proposed Trucking School located at 9426 Freeway Dr.

05/15/2023 Proposed signage for RestoDent Family Dentistry located at 8210 Macedonia Commons Blvd. unit #6.

05/15/2023 Proposed Façade for Sephora located at 500 E. Aurora Rd.

05/15/2023 Height Legislation Review.

The next regularly scheduled meeting is set for July 17, 2023.

All requests & documentation for the Planning Commission must be submitted by June 23, 2023

Tabled and Continued items will be removed after one (1) year of inactivity.



City of Macedonia
The Crossroads of Northeast Ohio
 9691 Valley View Road • Macedonia, Ohio 44056
 (330) 468-8360 • FAX (330) 468-8396

Building/Engineering/Zoning/Planning Department

**APPLICATION FOR HEARING BEFORE THE
 MACEDONIA PLANNING COMMISSION**

All Plans for Submittal Must be **FOLDED**.
 No Rolled Plans will be Accepted.

Address of Property Involved: 2055 Alexandria Way, Macedonia, OH 44056

Nature of Request: Zoning Certificate

Applicant Name: Peter C. Volas Phone: (216) 312-4255

Applicant Address: 9500 Euclid Avenue, HS1-02, Cleveland, OH 4419521

Applicant Email Address: Volasp@ccf.org

Applicant Signature: *Peter C Volas* Date: 5/16/23

Notes: Conditional zoning certificate to permit the medical use within the B-2 Community Shopping District.

Meeting Date: 300.00 Filing Fee: 50.00

Deadline for submitting applications is **21 DAYS** prior to the meeting date. When applying for a hearing, please furnish **THIRTEEN** sets of sketches, maps, drawings, descriptions, or photographs of the property in question. **THIRTEEN** copies of the site plans are required. **PLANS MUST BE FOLDED, NOT ROLLED.** No rolled plans will be accepted. If new construction is involved, the landscape and signage plan should be prepared. This application is for the purpose of scheduling and planning the time of the Macedonia Planning Commission. It is the Commissioner's desire to serve each applicant with a minimum of delay.

PLEASE NOTE: Permit fees are not included in the filing fee, additional fees may be required.

The Macedonia Planning Commission meets on the 3rd Monday of each month.

Make checks payable to:
 City of Macedonia

Please submit plans to:
 Macedonia Building Department
 9691 Valley View Road
 Macedonia, Ohio 44056

★ Service ★ Commitment ★ Pride ★

From: Grabnar, Daniel <GRABNAD@ccf.org>

Sent: Friday, May 19, 2023 8:38 PM

To: Bob Rodic <brodic@macedonia.oh.us>

Subject: Re: 2055 Alexandria Way - Occupancy Permit - Cleveland Clinic Signed Application

Hi Bob,

Apologies for this late submission, this email was in my drafts and I failed to send. Please find attached the signed Planning Commission Application to be included at the June 12, 2023 hearing. Peter Volas and myself will be there. See below a short summary to go along with the application:

Cleveland Clinic Foundation acquired Maguire Medical on April 4, 2023 and has signed a new 5-year lease with Maguire Medical Properties, LLC to continue operating as a Family Medicine Practice. Maguire Medical has been rebranded as Cleveland Clinic Family Medicine Nordonia Hills and we expect there to be no other impacts to any of our other Cleveland Clinic locations in Macedonia.

Have a great weekend and if you would please confirm receipt that we will be included in the 6/12/23 agenda.

Thank You

Daniel

Daniel Grabnar, MBA | Project Manager | Real Estate | Buildings + Design

Cleveland Clinic | 9500 Euclid Ave. | Cleveland, OH 44195

(216) 630-3299 | GRABNAD@ccf.org

Memorandum

TO: Nicholas Molnar, Mayor
and Macedonia Planning Commission

FROM: Brian M. Frantz, AICP

SUBJECT: Cleveland Clinic – Conditional Zoning Certificate

DATE: May 28, 2023

The applicant is proposing to occupy the Maguire office building located at 2055 Alexandria Way. The site is 1.415 acres and contains a 4,556 square foot building. The subject site is zoned B-2 Community Shopping District and medical offices are conditionally permitted.

I have reviewed an application dated May 16, 2023 in connection with this request and offer the Planning Commission with the following comments for their consideration:

Analysis

The building was approved in 2016 for a dual tenant facility and Dr. Maguire has since operated a conditionally permitted medical office pursuant to Section 1137.04 of the Planning and Zoning Code. Amongst other things, the Code requires basic information to be submitted with an application for a conditional permit. In this instance, the applicant has only submitted a short letter that details the acquisition of the Maguire practice by the Cleveland Clinic to continue operating a local family medicine practice. Understanding that the use is not going to change, certain things must be submitted for a permit. Specifically, a site plan and building floor plan to document the existing conditions must be submitted pursuant to Section 1137.04 (a) (1) (2) (A). However, I have no concerns with this applicant submitting the site plan previously used for the Maguire application. Finally, a written description of the intended hours of operation must be submitted in connection with this request.



Please note: a separate application for signage updates must be submitted to the Planning Commission for approval.

Recommendation

It's my understanding there will not be any alterations to the size of the tenant space or changes to the building. As such, it is reasonable to approve this application for a conditional zoning certificate subject to the applicant providing the necessary site plan and building floor plans specific to their occupancy. If the Commission finds it acceptable, the confirmation of the plan submittal can be conducted administratively to avoid the applicant attending an additional meeting.



City of Macedonia
Building, Engineering, Zoning & Planning Dept.
The Crossroads of Northeast Ohio

9691 Valley View Road °Macedonia, Ohio 44056
330 / 468-8360 ° Fax: 330 / 468-8396

**APPLICATION FOR HEARING BEFORE THE
MACEDONIA PLANNING COMMISSION**

ALL PLANS FOR SUBMITTAL MUST BE FOLDED. NO ROLLED PLANS WILL BE ACCEPTED.

DATE OF APPLICATION: 5/19/2023

LOCATION OF PROPERTY INVOLVED: 500 East Aurora Road

NATURE OF REQUEST: Planning Commission Approval

APPLICANT NAME & PHONE: Eric Greenberg 216-650-4170

APPLICANT ADDRESS: 3601 Green Road, Suite 302, Beachwood, Ohio 44122

APPLICANT EMAIL ADDRESS: egreenberg@siteconsult.com

APPLICANT SIGNATURE: _____

NOTES: SEEKING APPROVAL FOR MODIFICATIONS TO THE APPROVED NORDSTROM RACK ELEVATION, DUE TO UNKNOWN STRUCTURAL CONSTRAINTS.

MEETING DATE: 6/12/2023 **FILING FEE** _____ **ESCROW REQUIRED** _____

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Building/Engineering/Zoning/Planning Department

PLANNING COMMISSION REQUIREMENTS and MEETING DATES

- When applying for a hearing, please furnish TWO sets of sketches, maps, drawing, descriptions, or photographs of the property in question.
- TWO copies of site plans are required and to be FOLDED, not rolled. Rolled plans will not be accepted; 11 X 17 is preferred for Planning Commission review only.
- An electronic copy must be submitted to sroganish@macedonia.oh.us.
- If new construction is involved, a lighting and landscaping plan should be prepared and submitted. The signage plan should be prepared and submitted separately.
- Submittal by the due date does not guarantee a place on the agenda for the next meeting. The Building Commissioner determines the meeting date after reviewing the plans submitted.
- The applicant will be emailed or contacted by phone with additional fees that may be required.
- The applicant will be notified by email of meeting date and the agenda will be attached.
- The applicant may be emailed memos with comments from the City Planner, City Engineer, City Architectural Advisor, and the City Fire Inspector. These will be reviewed and discussed at the meeting. Revised plans will not be accepted after the City consultants memos have been distributed.
- Attendance to the meeting is not required, but highly recommended. (This is the chance to answer any questions that the Planning Commission might have).
- Permit fees are not included in Planning Commission fees and will be determined only after approval.

Please submit application and plans to:
Macedonia Building Department
9691 Valley View Road
Macedonia, Ohio 44056

Memorandum

TO: Nicholas Molnar, Mayor
and Macedonia Planning Commission

FROM: Brian M. Frantz, AICP

SUBJECT: Macedonia Gateway (Nordstrom Rack) – Design Amendments

DATE: May 28, 2023

I have reviewed an application dated May 19, 2023 (including architectural elevations) in connection with this request and offer the Planning Commission with the following comments for their consideration:

Background

Planning Commission approved the Nordstrom Rack elevations in 2022 and the applicant has since discovered that two of the approval stipulations require additional structural steel that is very costly to the project. As such, this request is to modify the 2022 approval by 1) reducing the height of the Route 8 parapet wall from six feet to four feet; and 2) extending the thin brick to the tower element and wrapping the corner over the roof.

Analysis

- 1) The four feet versus six-foot parapet wall along the Route 8 frontage is a reasonable request provided the roof top mechanical is adequately screened. The six-foot tall wall completely screens the mechanical units, whereas the four-foot-tall wall nearly screens everything. A series of three-dimensional views were provided to illustrate the four-foot parapet wall as seen from Route 8. In particular, a view from the Route 8/Route 82 intersection is provided to document the view of the mechanical units with the four-foot-tall wall because this is the only place where stopped traffic will see the building. Traffic moving from the south traveling north will pass the building at higher speeds and most likely won't see the negligible difference between four and six feet of wall height. According to the applicant, the six-foot parapet wall requires a fair amount of additional structural steel to brace the wall. This additional steel package is a significant cost compared to the little visual value the two extra feet provides.

In my opinion, even the two feet is drastic enough of a step-down that the Commission should be cognizant and attempt to reduce this height transition from the tower element. Given this, my strong preference is to lower the tower element by two feet to match the proposed reduction along the Route 8 frontage. I believe this approach will ensure visual symmetry of the wall height transition from the tower element to the parapet wall.

- 2) Extension of the thin brick to the tower element is an additional cost savings measure being proposed. However, I am concerned that the financial benefit in this instance is small

compared to the overall design impact being proposed. The tower element was heavily discussed and, in my opinion, should be maintained as approved. Based on my discussions with the applicant, this portion of the request yields a smaller financial benefit when compared to the savings from the Route 8 frontage proposal.

Conclusion

I appreciate the need to value engineer the building to reduce budget given material costs and rising interest rates. However, this is a significant project for the applicant, but also for the City. As such, now is the time to ensure the City obtains the best possible architectural improvement while balancing the financial interests of the private investor. Therefore, I believe it is reasonable to approve the Route 8 wall height reduction with two stipulations:

- 1) The applicant ensures any portion of the rooftop equipment exposed above the four-foot parapet wall is properly screened; and
- 2) The height of the main tower element is lowered by two feet to ensure visual symmetry with the reduced parapet wall height.

Regarding the request to utilize thin brick on the tower element, I believe the Commission should maintain the full masonry brick as previously approved.

Please feel free to contact me if you have any questions or need additional information.

MEMORANDUM

To: Mr. Nicholas Molnar, Mayor
Mr. Bob Rodic, Building Commissioner

From: Pam Schultz, Architectural Review Consultant

Subject: Sephora/Nordstrom Rack/Entire Shopping Center
500 East Aurora Road
Northfield Center, OH

Date: May 31, 2023

I have reviewed the submitted REVISED plans/continued changes for Sephora/Nordstrom Rack/Shopping Center. Please reference my previous review dated May 4th2023. This review has the listed criteria sections to reference.

Overall, the tenant has not addressed the comments made in previous reviews. In fact, I believe they have taken a step backwards.

The Side elevation facing Route 8 MUST hide all the HVAC equipment. It is required by code and must be addressed! I believe the proposed lower parapet wall on Route 8 still allows the HVAC equipment to be visible and will allow the structural supports needed for the Nordstrom Rack signage to be visible. With this comment, the height of the façade next to the tower is not at the same height as the Nordstrom Rack façade. The different heights on either side of the tall tower needs to be addressed. The Nordstrom Rack signage height, the tower height and the Route 8 wall/façade all need to be addressed and finalized before construction begins.

In previous reviews, I also commented on the panels placed behind the signage. The concrete panels should be placed in a brick pattern instead of the stacked pattern as shown. The brick pattern would be more in line with the Western Reserve Criteria. (please see #7 on review dated May 4th)

The building is located in the Town Center area which is governed by the Architectural Design Standards of Macedonia, OH along with specifically, 1172.03 of the Applicable Town Center Standards. These standards are set in place for the entire community. Making substitutions or allowances for an individual tenant would open up MAJOR issues for any current tenant which wasn't given approval for their "branding" design (IHOP's blue roof, Taco Bell's purple lights) as well as for future tenants who believe their "branding" allows exceptions to be made. With this in mind, again, the façade cannot be approved with the black and white columns.

Recommendation:

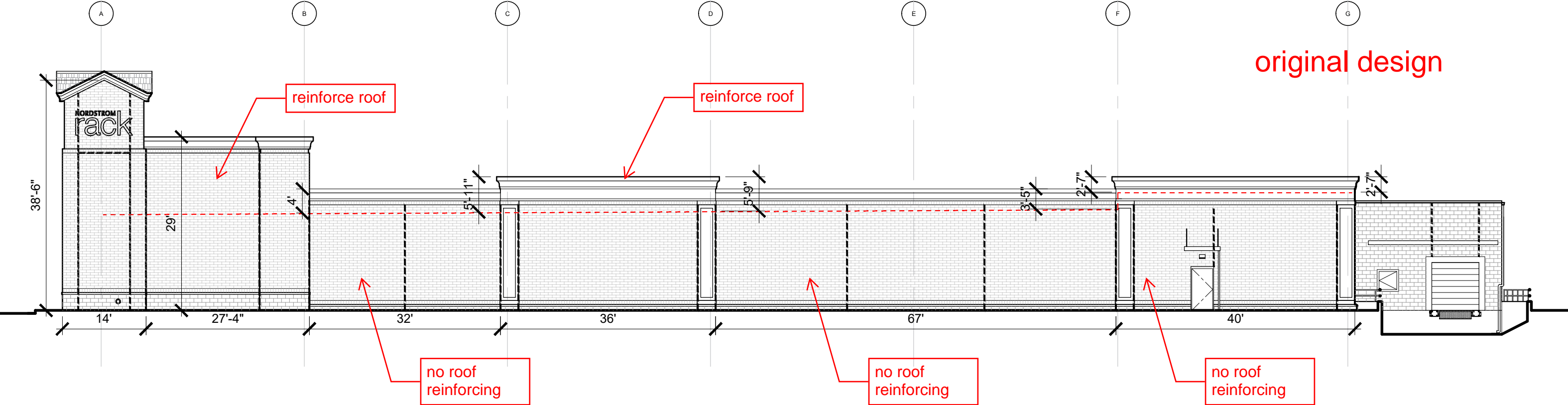
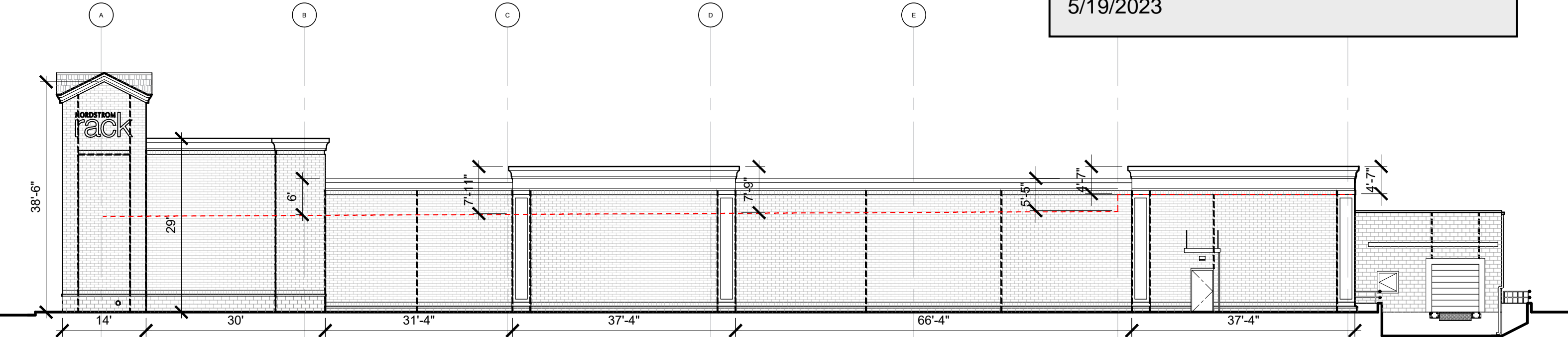
If the commission agrees, it is my strong belief this shopping center façade cannot be approved as shown. Further review is needed to verify the owner is in compliance with the previously agreed upon approvals for the overall design. The signage, the Route 8 façade and the Sephora façade materials need addressed. It is my recommendation the plans need to be tabled until further discussion about code requirements.

Please feel free to contact me with any questions or comments. Thank you!
Pam Schultz

MACEDONIA GATEWAY

FACADE RENOVATION

NORDSTROM RACK - ELEVATION STUDY
5/19/2023



original design

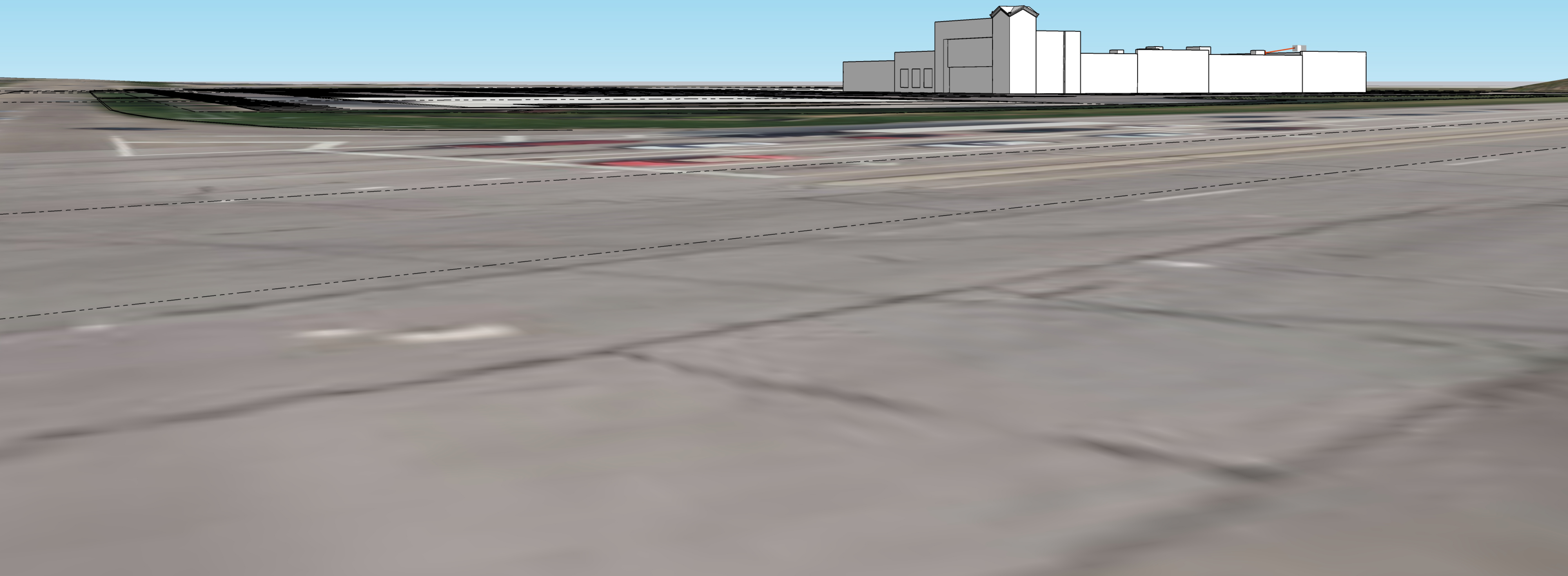
reduced parapet design

MACEDONIA GATEWAY

FACADE RENOVATION

NORDSTROM RACK - ELEVATION STUDY

5/19/2023



MACEDONIA GATEWAY

FACADE RENOVATION

NORDSTROM RACK - ELEVATION STUDY

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FACADE RENOVATION

NORDSTROM RACK - ELEVATION STUDY

5/19/2023



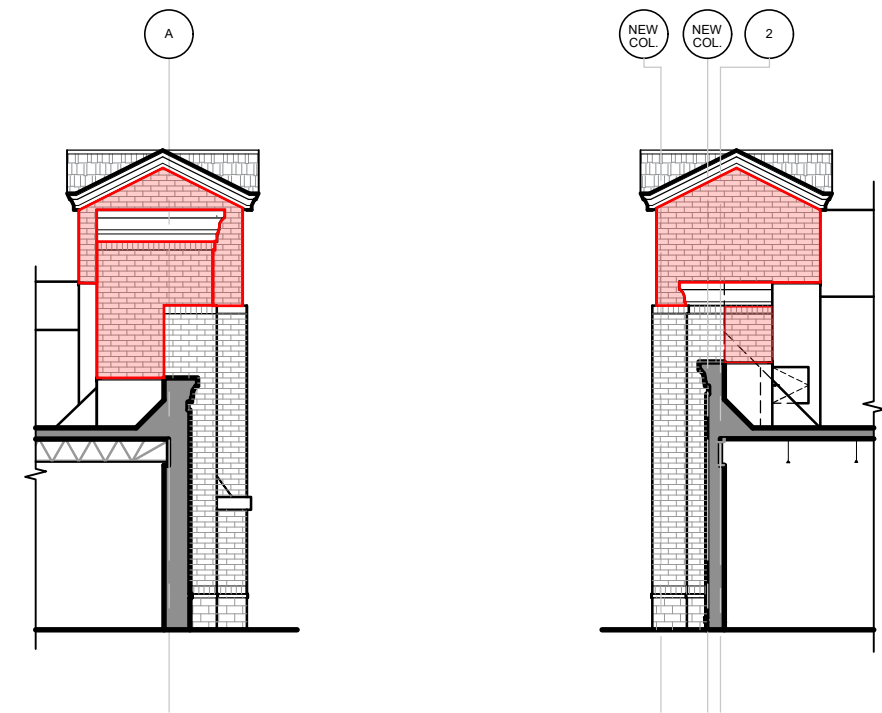
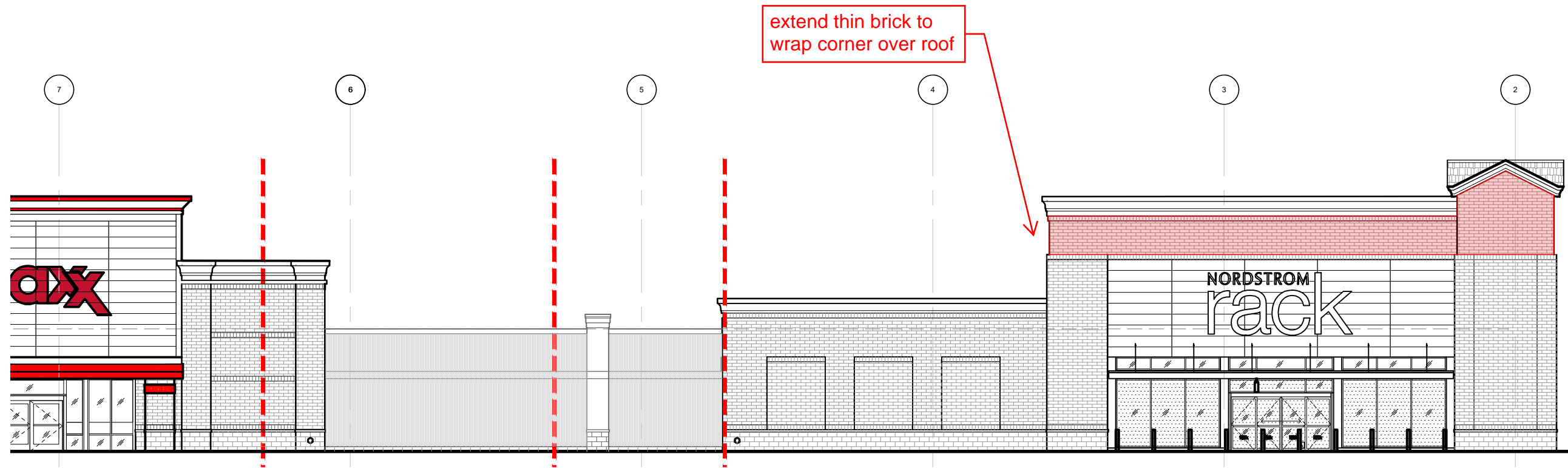
MACEDONIA GATEWAY

FACADE RENOVATION

NORDSTROM RACK - ELEVATION STUDY

5/19/2023





MACEDONIA GATEWAY
FACADE RENOVATION

NORDSTROM RACK - ELEVATION STUDY
5/19/2023



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**APPLICATION FOR HEARING BEFORE THE
MACEDONIA PLANNING COMMISSION**

ALL PLANS FOR SUBMITTAL MUST BE FOLDED. NO ROLLED PLANS WILL BE ACCEPTED.

DATE OF APPLICATION: 5/19/2023

LOCATION OF PROPERTY INVOLVED: 500 East Aurora Road

NATURE OF REQUEST: Planning Commission Approval

APPLICANT NAME & PHONE: Craig Cospers (216) 524-8400

APPLICANT ADDRESS: 6060 Rockside Woods Blvd. North, Suite 100, Independence OH 44131

APPLICANT EMAIL ADDRESS: ccospers@cuparc.com

APPLICANT SIGNATURE: 

NOTES: SEEKING APPROVAL FOR THE NEW RETAIL TENANT SEPHORA & THE ADJACENT FUTURE TENANT ELEVATION. THE SCOPE INCLUDES A MODIFICATION TO THE EXISTING FACADE AT THE FORMER BURLINGTON COAT
FACTORY ENTRANCE. THE SITE LIGHTING AND LANDSCAPING FOR GATEWAY MACEDONIA WAS APPROVED AT
THE 12/20/2021 MEETING AND IS NOT CHANGED FOR THIS TENANT. NOT INCLUDED IN THE SCOPE ARE THE
TENANT SIGNAGE, WHICH WILL BE SUBMITTED INDEPENDENTLY

MEETING DATE: 6/12/2023 FILING FEE _____ ESCROW REQUIRED _____

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Macedonia, Ohio 44056

Memorandum

TO: Nicholas Molnar, Mayor
and Macedonia Planning Commission

FROM: Brian M. Frantz, AICP

SUBJECT: Macedonia Gateway (Sephora) – Façade Improvements (2nd Review)

DATE: May 29, 2023

I have reviewed an application dated May 19, 2023 (including architectural elevations) in connection with this request and offer the Planning Commission with the following comments for their consideration:

Background

The applicant met with the Planning Commission in May to discuss initial concepts for the Sephora tenant space. Several design items were discussed with the applicant and resulting from those conversations the proposed plans were submitted for final approval.

Analysis

- 1) The revisions address the uniformity in colors compared to the other retail units. The Sephora tenant space is now shown as China White with black sign lettering.
- 2) The black store front columns for Sephora were agreed upon by the Commission during the May meeting as a compromise to the applicant agreeing to matching the China White as used on the other tenant spaces previously approved.
- 3) There was discussion about the misalignment of the front door for the two units. However, the applicant explained that the doors are aligned with the center of the entire unit as opposed to the frontage glass. Two options have been presented (see Sheets 4A and 4B) for the door location to address the alignment concerns. Sheet 4A details a single door that can be located on either side of the column (as determined by the tenant) and Sheet 4B illustrates a double door to the far west of the tenant space (as determined by the tenant). From a visual standpoint, I believe the first option on Sheet 4A should be required, with the door to the right of the column as opposed to the left.
- 4) The black vertical bar(s) adjacent to the doors have been changed to aluminum to match other tenant spaces.
- 5) As discussed in the previous meeting, the existing Nichiha cornice transitioning from the Nordstrom Rack unit has been addressed.

Conclusion

The applicant has done a nice job addressing the concerns noted by the Commission and this proposal should be considered for approval. Please feel free to contact me if you have any questions or need additional information.

MEMORANDUM

To: Mr. Nicholas Molnar, Mayor
Mr. Bob Rodic, Building Commissioner

From: Pam Schultz, Architectural Review Consultant

Subject: Sephora
500 East Aurora Road
Northfield Center, OH

Date: May 4, 2023

I have reviewed the submitted revised plans for Sephora.

The proposed plans are for revisions to the new facade in the former Burlington Shopping Center. This building is located in the Town Center area which is covered by the Architectural Design Standards of Macedonia, OH along with specifically, 1172.03 of the Applicable Town Center Standards. With those standards in mind, I offer the following for your consideration:

1. The most significant comment is the façade cannot be approved as shown. The building materials do not adhere to the standards set forth in the above listed and noted code.
2. After significant review, the drawings of the overall façade do not include comments from previous tenant submissions and adds to the confusion of what is being presented. The overall design of the shopping center must be addressed. (One drawing shows a small tower,(page 4), one elevation on the same page eliminates the tower. In the wall sections (page 5), it is unclear if the side elevation of this façade will be visible due to the low height of the wall between Nordstrom Rack and “Tenant”. The red trim shown on the TJMaxx façade was not approved. . . etc.
3. The Sephora and “Tenant” facades are dwarfed next to the TJ Maxx and Nordstrom Rack. The overall look of the shopping center should be addressed before individual tenant facades. (see previous comments on Memo dated November 7, 2022 and December 5, 2022 where canopy heights were addressed.
4. The entry door at the glass entry system of both Sephora and “Tenant” should be centered
5. Material choices and colors aren’t clear. Paint color 1 location? Paint color 2 location? Thin brick is not approved. Black and white storefront system is not approved. (This type of storefront doesn’t meet the Architectural Design Criteria of Macedonia). The overall look of the shopping center should be addressed with the base of the building using related/similar colors and materials.
6. Please review previous comment regarding the visibility of the side of the façade. PREVIOUS COMMENT: Please verify the side view of the façade is not visible from Route 8/West elevation. If it is visible, verify the side details are appropriately simple and colors allow the side details to blend with the actual façade. All rooftop angle iron or any mechanical equipment must also be hidden from view. This is a highly visible area from Route 8, please verify compliance.

Page Two

**Memo regarding Sephora/Burlington Shopping Center
May 4, 2023**

7. The panels or background of the signage areas should match/coordinate. Please see the comment regarding Nordstrom Rack. . . "The Cast Stone detail is appreciated due to it's "Sandstone/Limestone" look. Please visually verify. The cement panels located behind the Nordstrom Rack signage is placed in a contemporary pattern. The placement of the cement panels in a brick pattern would give toward a classic sandstone/limestone look that adheres to the Western Reserve Criteria set forth in j1172.02(a)(1)(2)(3) and 1172.03. Large expanses of Dryvit are out of proportion with the surrounding tenants and is not permitted. (Please see 1172.02 (a)(5) and (10)(D). The signage area proposes a "Cementitious Panel System to replace dryvit. Careful consideration is needed to create the sandstone/limestone look. Please minimize dryvit where possible. (please see 1172.02(10)(D) and 1172.03((2)(C)and (10)(e). In addition, please verify the color of material used matches adjacent tenants. (neighboring tenants may have used white)."

Recommendation:

Overall, it is my recommendation, the material choices submitted for the Sephora store do not meet the requirements of the Macedonia Design Standards.

In addition, If the commission agrees, it is my strong belief this shopping center needs further review to verify the owner is in compliance with the previously agreed upon approvals for the overall design before individual tenants submit for approval.

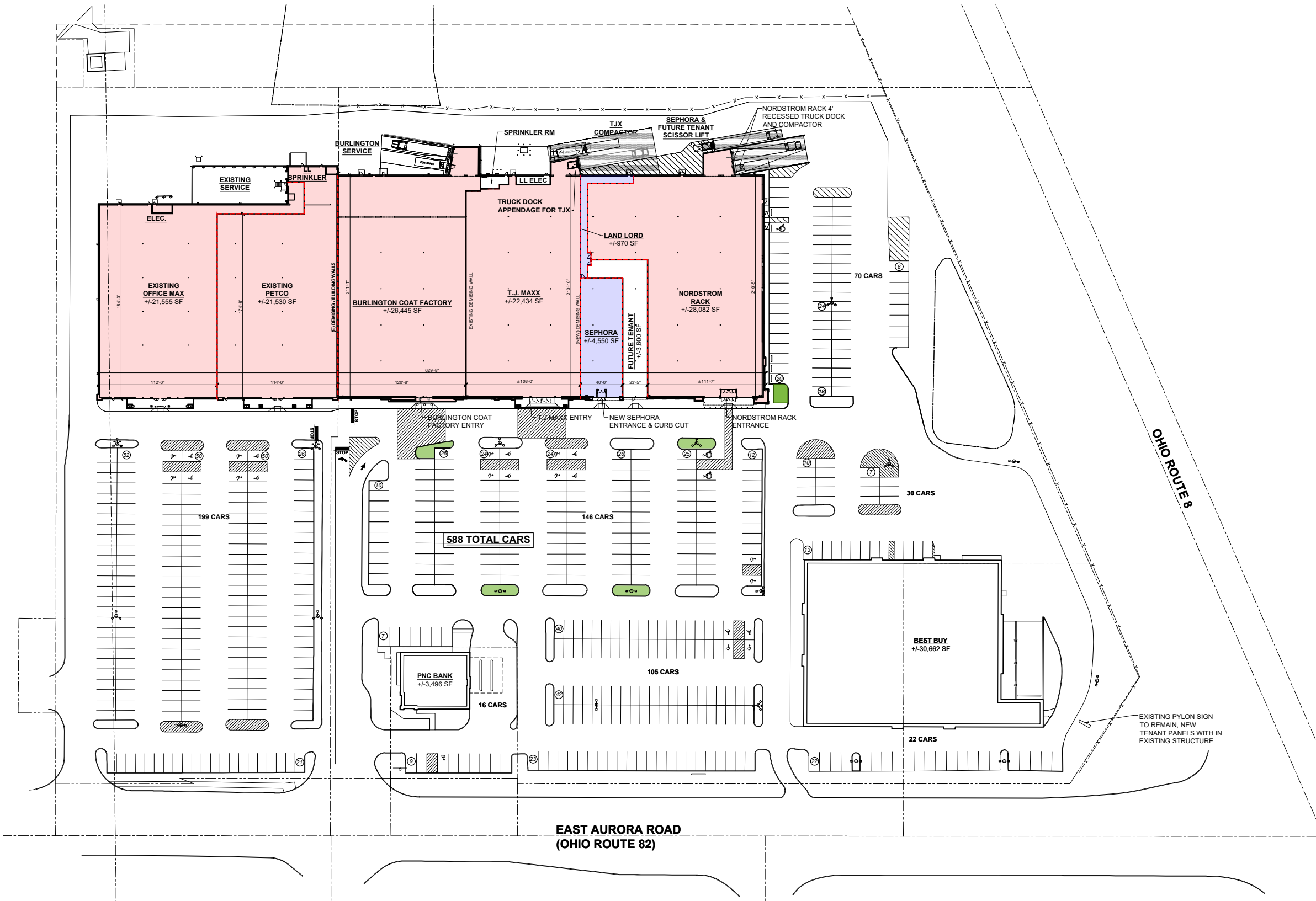
Please feel free to contact me with any questions or comments.

Thank you!

Pam Schultz

MACEDONIA GATEWAY

MACEDONIA, OH



BUILDING AREAS	
TENANT	SF
OFFICE MAX	21,555
PETCO	21,530
BURLINGTON COAT FACTORY	26,445
T.J. MAXX	22,434
SEPHORA	4,550
FUTURE TENANT	3,600
NORDSTROM RACK	28,082
SUB TOTAL	128,196
BEST BUY	30,662
PNC BANK	3,496
TOTAL	162,354
PARKING SUMMARY	
PARKING PROVIDED	SPACES
OFF STREET	588
TOTAL	588
PARKING RATIO	SPACES PER 1000 SF
588 SPACES / 162.35 =	3.62
MACEDONIA REQUIRED RATIO	1 CAR / 250 SF
TOTAL BUILDING SF	162,354
SF REDUCED BY 20% FOR STORAGE	
PER 1171.11 (b)(2)	129,883
129,883 SF / 250 SF =	519 CARS REQUIRED
	588 CARS PROVIDED

NOTE:
SITE LIGHTING AND LANDSCAPING WERE APPROVED AT 12/20/21 PLANNING COMMISSION MEETING

- SEPHORA & LANDLORD CORRIDOR
- EXISTING / UNDER CONSTRUCTION TENANTS
- FUTURE TENANT (TBD)

SITE PLAN

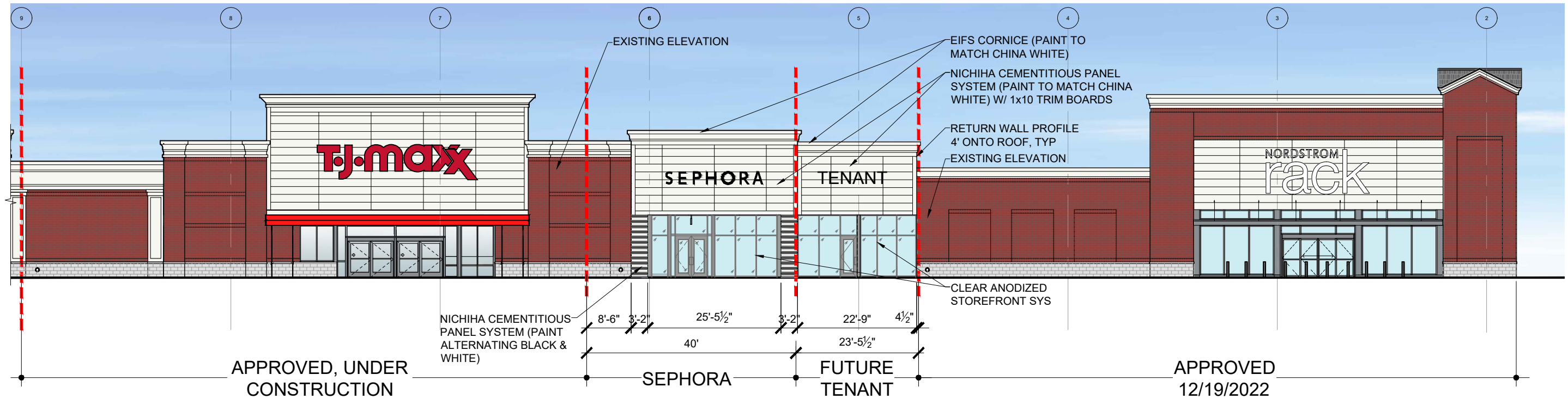
MAY 19, 2023
20-031

Design information herein is property of CUPKOVIC architecture, llc
Architect

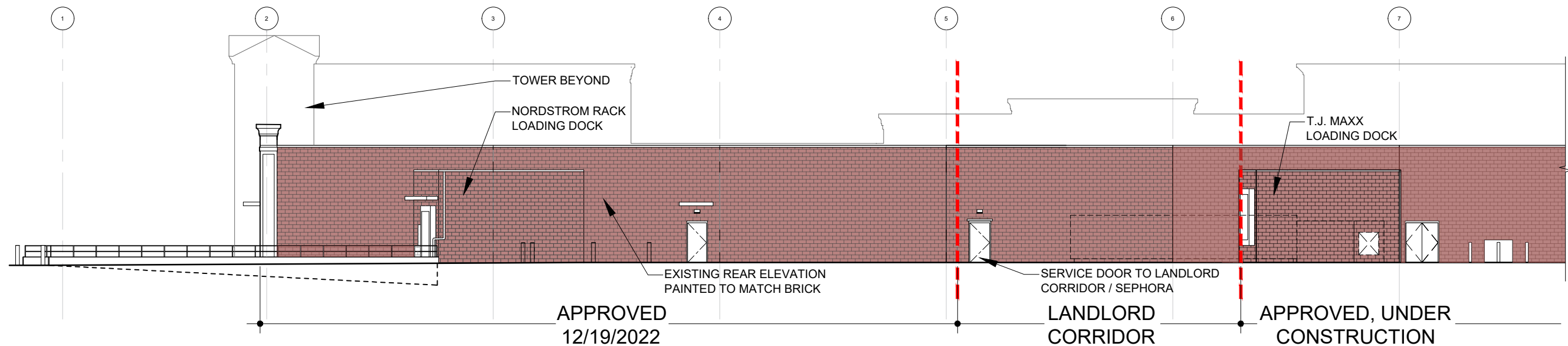




OVERALL NORTH ELEVATION



ENLARGED PARTIAL NORTH ELEVATION



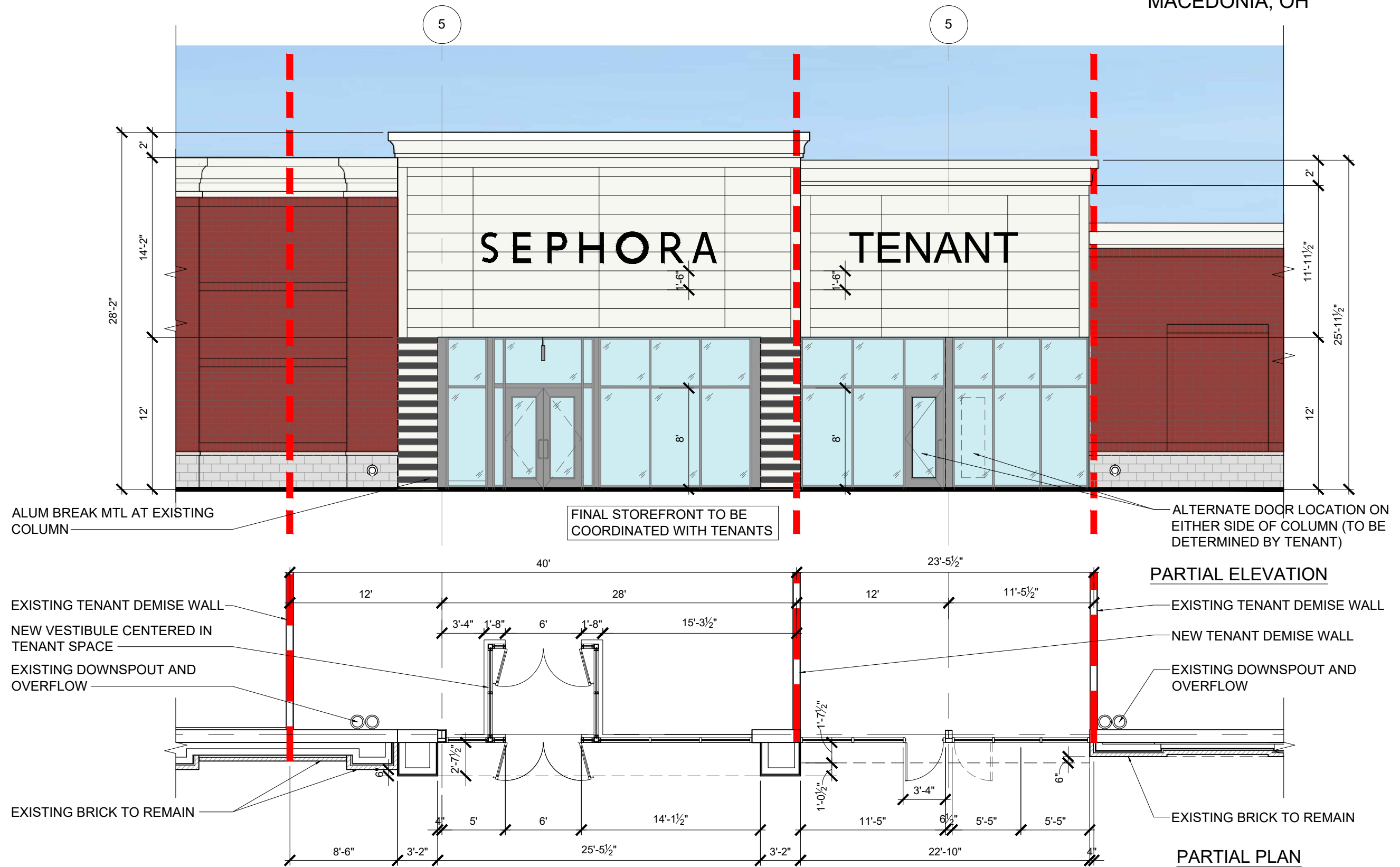
ENLARGED PARTIAL SOUTH ELEVATION

PROPOSED ELEVATIONS



Design information herein is property of CUPKOVIC architecture, llc
Architect

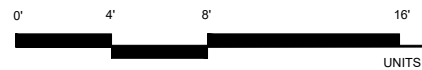
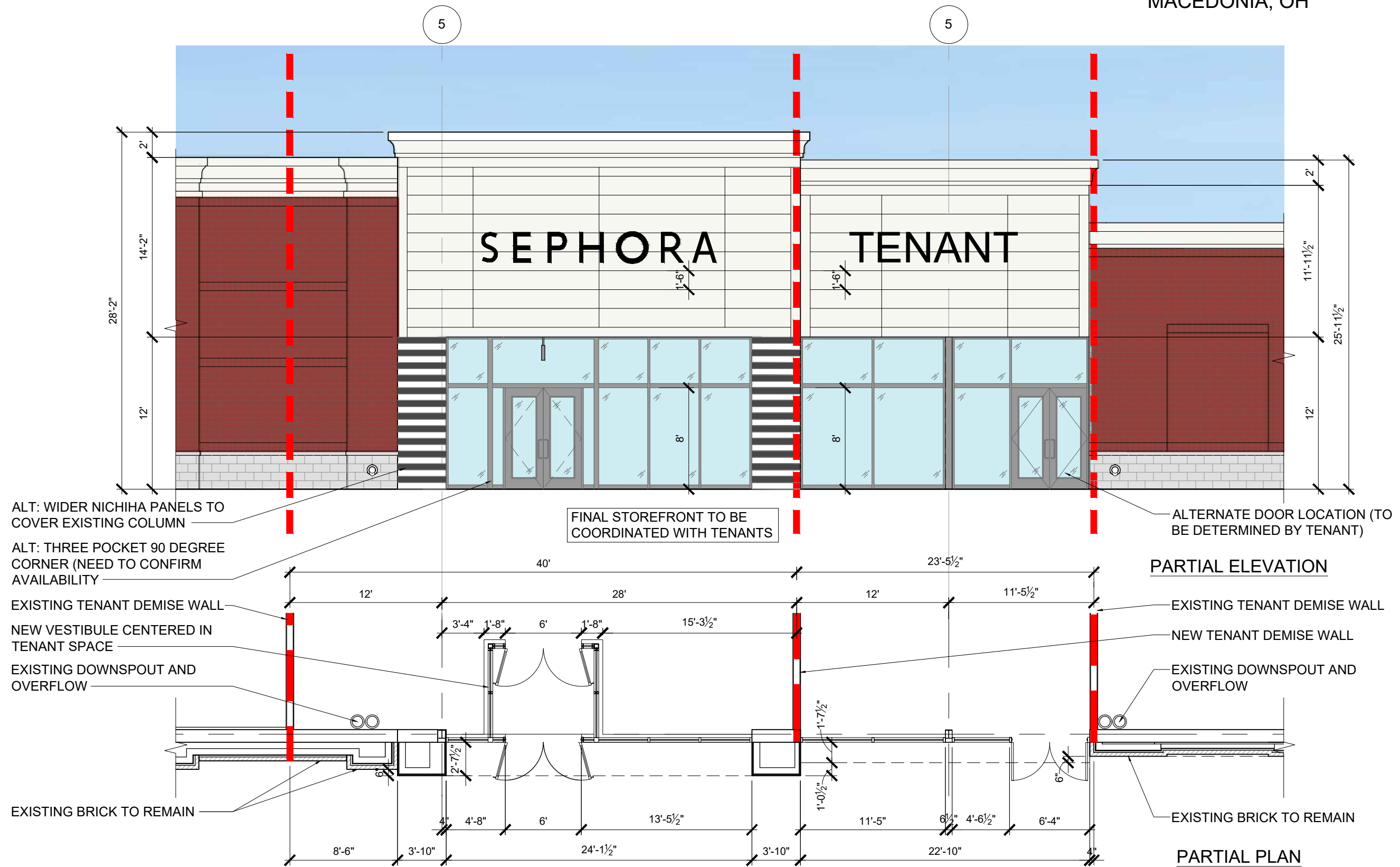
MAY 19, 2023
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ENLARGED PARTIAL PLAN & ELEVATION - OPTION 1

Design information herein is property of CUPKOVIC architecture, llc
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MAY 19, 2023
20-031



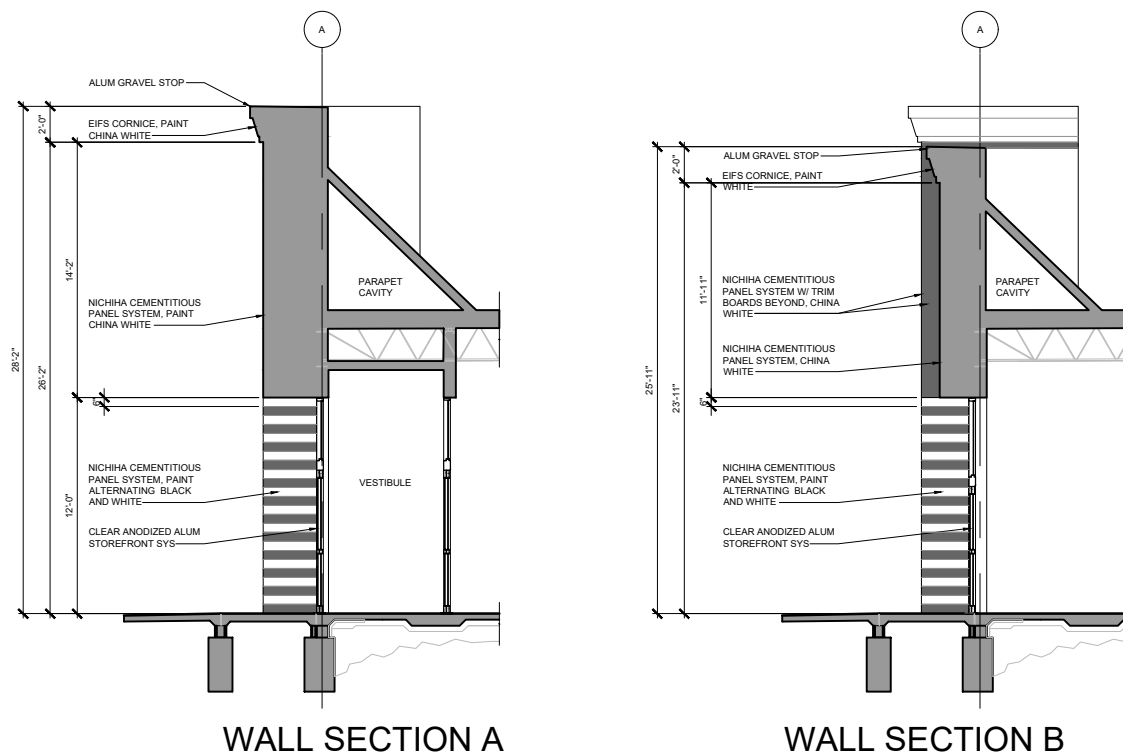
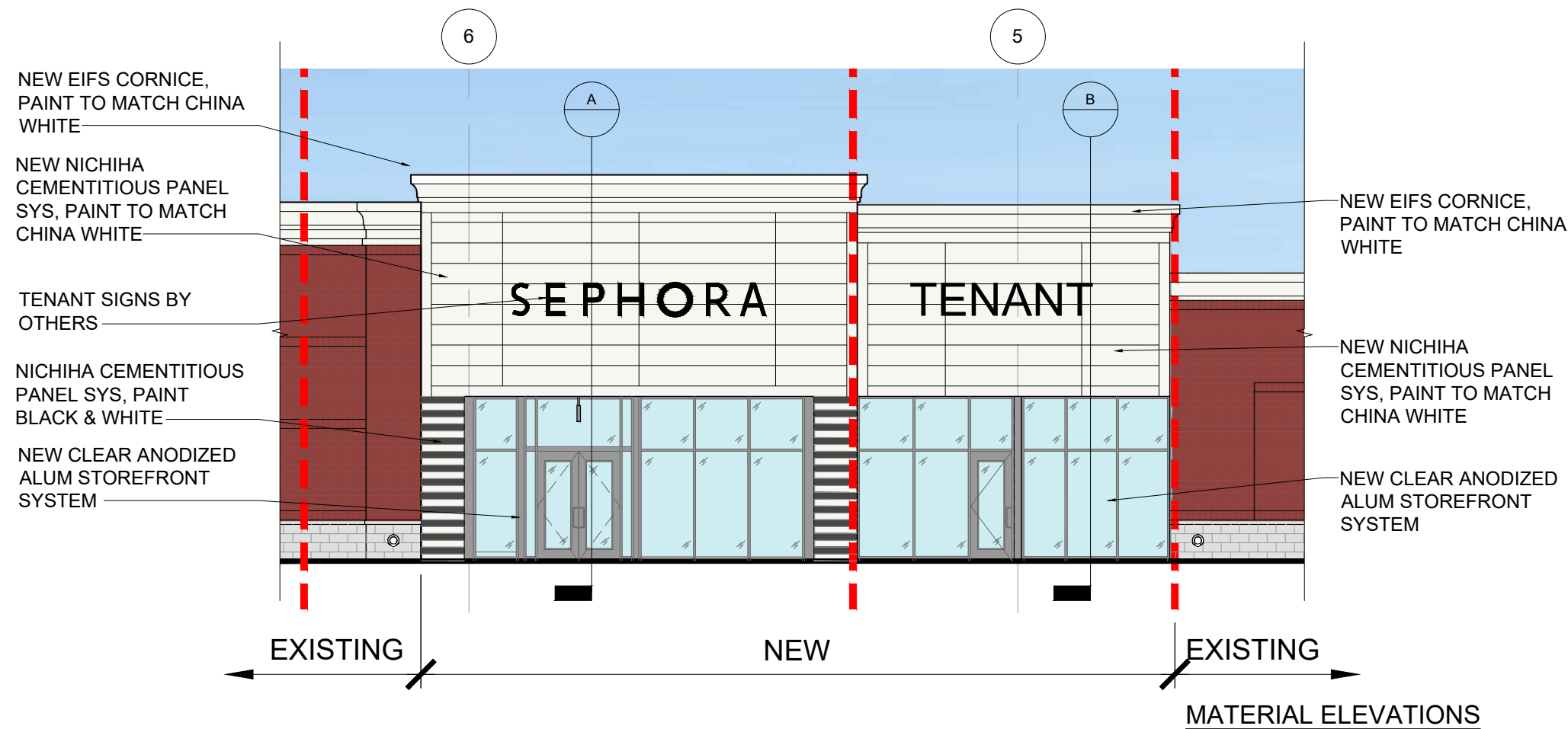
ENLARGED PARTIAL PLAN & ELEVATION - OPTION 2

Design information herein is property of CUPKOVIC architecture, llc
 Architect


MAY 19, 2023
 20-031



EXAMPLE ELEVATION



PRE-FINISHED ALUM GRAVEL STOP WITH CONTINUOUS CLEAT
NEW STOREFRONT SYSTEM AND GLAZING CLEAR ANODIZED ALUMN FINAL LAYOUT BY SEPHORA
EIFS CORNICE MRF: STO COLOR: PAINT TO MATCH DRYVIT CHINA WHITE
PAINT MRF: BENJAMIN MOORE COLOR: PAINT TO MATCH DRYVIT CHINA WHITE
PAINT MATCH BRICK (COLOR TBD)

 <p>NICHHA CEMENTITIOUS PANELS LATURA V-GROOVE PAINT BLACK PER SEPHORA</p>	 <p>PAINT 1 COLOR: PAINT TO MATCH DRYVIT CHINA WHITE</p>
 <p>NICHHA CEMENTITIOUS PANELS LATURA V-GROOVE PAINT TO MATCH DRYVIT CHINA WHITE</p>	 <p>PAINT 2 COLOR: MATCH BRICK (REAR ELEVATION)</p>

EXTERIOR MATERIALS & WALL SECTIONS

Design information herein is property of CUPKOVIC architecture, llc
Architect

MAY 19, 2023
20-031



City of Macedonia

The Crossroads of Northeast Ohio

9691 Valley View Road • Macedonia, Ohio 44056
(330) 468-8360 • FAX (330) 468-8396

Building/Engineering/Zoning/Planning Department

PLANNING COMMISSION REQUIREMENTS and MEETING DATES

1. [Macedonia Planning Commission Application](#) and fee.
2. When applying for a hearing, please furnish TWO sets of sketches, maps, drawings, descriptions, or photographs of the property in question.
3. **TWO** copies of site plans are required and are to be **FOLDED**, not rolled. Rolled plans will not be accepted; 11x17 is preferred for Planning Commission review only.
4. An electric copy must be submitted to sroganish@macedonia.oh.us.
5. If new construction is involved, a lighting and landscape plan should be prepared and submitted. The signage plan should be prepared and submitted separately.
6. Submittal by the due date does not guarantee a place on the agenda for the next meeting. The Building Commissioner determines the meeting date after reviewing the plans submitted.
7. The applicant will be emailed or contacted by phone with additional fees that may be required.
8. The applicant will be notified by email of the meeting date and the agenda will be attached.
9. The applicant may receive emailed memos with comments from the City Planner, City Engineer, City Architectural Advisor, and the City Fire Inspector. These documents will be reviewed and discussed at the meeting.
10. Attendance at the meeting is not required, but highly recommended. This is the opportunity to answer any questions that the Planning Commission might have.
11. Permit fees are not included in Planning Commission fees and will be determined only after approval.

Please submit application, plans, and payment to:

Macedonia Building Department
9691 Valley View Road
Macedonia, Ohio 44056



City of Macedonia

The Crossroads of Northeast Ohio

9691 Valley View Road • Macedonia, Ohio 44056
(330) 468-8360 • FAX (330) 468-8396

Building/Engineering/Zoning/Planning Department

APPLICATION FOR HEARING BEFORE THE MACEDONIA PLANNING COMMISSION

All Plans for Submittal Must be **FOLDED**.
No Rolled Plans will be Accepted.

DATE OF APPLICATION: May 18, 2023

LOCATION OF PROPERTY INVOLVED: 8213 Golden Link Blvd, Macedonia, OH 44067

NATURE OF REQUEST: Site Plan Approval

APPLICANT NAME: Jake Panter - Approved Representative PHONE: 630-487-3432

APPLICANT ADDRESS: 3875 Embassy Parkway, Suite 280, Akron, OH 44333

APPLICANT EMAIL ADDRESS: jake.panter@kimley-horn.com

APPLICANT SIGNATURE:  DATE: May 18, 2023

NOTES: See Project Narrative for more information on the project.

MEETING DATE: June 12th FILING FEE: \$150 ESCROW REQUIRED:

Deadline for submitting applications is **21 DAYS** prior to the meeting date. When applying for a hearing, please furnish **TWO** sets of sketches, maps, drawings, descriptions, or photographs of the property in question. **TWO** copies of the site plan are required. **PLANS MUST BE FOLDED, NOT ROLLED.** No rolled plans will be accepted. 11x17 is acceptable for Planning Commission review only. If new construction is involved, the landscape and lighting plan should be prepared. This application is for the purpose of scheduling and planning the time of the Macedonia Planning Commission. It is the Commissioner's desire to serve each applicant with a minimum of delay.

PLEASE NOTE: PERMIT FEES ARE NOT INCLUDED IN THE FILING FEE, ADDITIONAL FEES MAY BE REQUIRED.

The Macedonia Planning Commission meets on the 3rd Monday of each month.

Make checks payable to:
City of Macedonia

Please submit plans to:
Macedonia Building Department
9691 Valley View Road
Macedonia, Ohio 44056



City of Macedonia

The Crossroads of Northeast Ohio

9691 Valley View Road • Macedonia, Ohio 44056
(330) 468-8360 • FAX (330) 468-8396

Building/Engineering/Zoning/Planning Department

PLANNING COMMISSION

Meeting Dates

2023 MEETING DATES	DEADLINE FOR SUBMITTAL
January 9, 2023	December 16, 2022
February 13, 2023	January 20, 2023
March 20, 2023	February 24, 2023
April 17, 2023	March 24, 2023
May 15, 2023	April 21, 2023
June 12, 2023	May 19, 2023
July 17, 2023	June 23, 2023
August 21, 2023	July 28, 2023
September 18, 2023	August 25, 2023
October 16, 2023	September 22, 2023
November 20, 2023	October 27, 2023
December 18, 2023	November 22, 2023

ALL APPLICATIONS MUST BE SUBMITTED BY THE DEADLINE: NO EXCEPTIONS

ALL SETS MUST INCLUDE:

2 Copies of 11 x 17 plans

Planning Commission Application with payment

Contact information and emails for those to be notified of meeting or any changes

Memorandum

TO: Nicholas Molnar, Mayor
and Macedonia Planning Commission

FROM: Brian M. Frantz, AICP

SUBJECT: Chick-Fil-A –Drive-Thru Modification

DATE: June 2, 2023

Chick-Fil-A is proposing to modify once again the existing drive-thru for their restaurant located on outlot 6A of the Crossings at Golden Link. Based upon my review of an application dated May 18, 2023, I offer the following comments for Planning Commission's consideration of the request:

Analysis

The entire purpose of this project is to increase access into the drive-thru by creating three stacking lanes as opposed to the two currently provided. According to a traffic study conducted by Kimley Horn, user utilization of this restaurant is 83% drive-thru and only 17% dine-in. To address this statistic, several site modifications are needed. Boiling the request down, essentially three things will occur if the Commission approves the request.

- (1) **Parking Reduction** – 13 parking spaces are proposed for elimination. This will leave the site with 49 parking spaces compared to the 62 that currently exist.

COMMENTS: Drive-thru utilization has increased significantly since COVID-19. The site will remain compliant with the number of required parking spaces and therefore the reduction is not a concern.

- (2) **Access Impacts** – More than half the southerly parking row adjacent to Chipotle is proposed for elimination to accommodate the third stacking lane into the drive-thru. I have concern regarding how the transition from three lanes into two occurs. Moreover, I am concerned with the proposal to install permanent bollard posts blocking off cross-access from Chipotle to Chick-Fil-A.

COMMENTS: Elimination of the cross-access has surfaced several times with the Chick-Fil-A site in the past. Cross-access is required by the Criteria for Development of Outlots. As such, the applicant must determine the approval process to eliminate this development feature. Additionally, the applicant must discuss this proposal with the Chipotle ownership to ensure they are supportive of the request. Documentation of Chipotle's response should be provided in writing.

- (3) **Landscaping Removal** – To effectuate the change, some landscaping is proposed to be removed.

COMMENTS: It appears the percentage of required landscaping is being met, but it's not clear what landscape material is being eliminated and if that impacts compliance

with the Criteria for Development of Outlots. The plans should be modified to clearly articulate if all the standards of the Criteria for Development of Outlots are being met with this proposal.

Conclusion

Chick-Fil-A continues to be very successful in the City of Macedonia. But it is apparent that the site is too small for their continued growth. Cross-access between the properties is required by the Criteria for Development of Outlots and elimination of this feature must be appropriately addressed. The applicant needs to determine the approval process to eliminate cross-access and seek written confirmation that this plan is acceptable to the owners of Chipotle. At this time, I don't believe the proposal can be approved by the Commission without this information.

MEMO

To: Planning Commission
From: Joe Gigliotti, P.E.
Date: 06-01-2023
Re: Plan Review Comments: Chick-fil-A drive thru

We have reviewed the plans submitted for the above-named project, plans dated 02-23-2023. We offer the following comments:

1. The plans submitted shows a drive apron of approximately 48 feet wide. Macedonia code indicates a maximum drive apron width of only 36 feet.
2. The current "wrap-around" style of vehicle stacking for the drive-thru has prevented vehicle stacking onto Golden Link Blvd. (most important), and would certainly provide more stacking for a larger number of vehicles than the proposed condition. Reducing the amount of stacking would not be recommended.

PRIVATE IMPROVEMENT PLANS FOR CHICK-FIL-A

8213 GOLDEN LINK BLVD,
MACEDONIA, OH 44067



Chick-fil-A
5200 Buffington Road
Atlanta, Georgia 30349-2998

Kimley»Horn

© 2023 KIMLEY-HORN AND ASSOCIATES, INC.
111 WEST JACKSON BLVD., SUITE 1320, CHICAGO, IL 60604
PHONE: 312-728-9445
WWW.KIMLEY-HORN.COM

CHICK-FIL-A
MACEDONIA FSU
8213 GOLDEN LINK BLVD
MACEDONIA, OH 44067

FSR#2216

REVISION SCHEDULE
NO. DATE DESCRIPTION

CONSULTANT PROJECT # 268214000
PRINTED FOR JHP
DATE 02/23/2023
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SHEET
COVER SHEET

SHEET NUMBER

C000

LEGEND

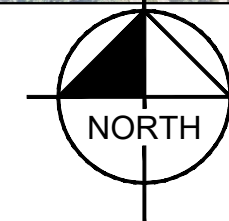
	PARCEL PROPERTY LINE
	60' BUFFER PARK AND BUILDING SETBACK
	PAVEMENT SAWCUT AND JOIN LINE
	APPROXIMATE LIMIT OF DEMOLITION
	PROPOSED CONTOUR
	EXISTING CONTOUR
	DEMOLITION AREA
	LANDSCAPE
	REMOVE AND REPLACE ASPHALT PAVEMENT
	CONCRETE SIDEWALK
	REMOVE AND REPLACE CONCRETE PAVEMENT

SCOPE OF WORK

CHICK-FIL-A INTENDS TO MODIFY THE CURRENT PARKING LOT TO BETTER ACCOMMODATE THE DRIVE-THRU LANES. THESE DRIVE-THRU MODIFICATIONS WILL INCLUDE PARKING STALL REALIGNMENT AND STRIPING MODIFICATIONS. WITH THE MODIFICATIONS TO THE DRIVE-THRU LANES, IT IS ANTICIPATED TO IMPROVE THE EXISTING QUEUING AND TRAFFIC FLOW. NO INTERIOR IMPROVEMENTS ARE INCLUDED IN THIS SCOPE OF WORK. ALL DISTURBED LANDSCAPING WILL BE RESTORED.



VICINITY MAP
NTS



SHEET INDEX	
SHEET NO.	DESCRIPTION
C000	COVER SHEET
1 OF 1	EXISTING CONDITIONS
C100	DEMOLITION PLAN
C101	EROSION CONTROL PLAN
C102	EROSION CONTROL DETAILS
C200	SITE PLAN
C201	DETAILED SITE PLAN
C300	DRAINAGE AND GRADING PLAN
C301	GRADING PLAN (ENLARGED)
C400	CIVIL DETAILS
C401	CIVIL DETAILS
C402	CIVIL DETAILS
C500	GENERAL NOTES

PROJECT TEAM

OWNER/DEVELOPER KAYLA FORTNER, PE DEVELOPMENT MANAGER JLL, on behalf of CHICK-FIL-A 5200 BUFFINGTON ROAD ATLANTA, GA 30349 (404) 824-3616	SURVEYOR STEVE W. CLUTTER, P.S. CESO INC. 175 MONTROSE WEST AVE, SUITE 400 AKRON, OH 44321 (330) 396-5967
DAN STANKO STORE OPERATOR CHICK-FIL-A 8213 GOLDEN LINK BLVD MACEDONIA, OH 44067 (216) 973-9688	CIVIL ENGINEER PRIMARY CONTACT: JAKE PANTER KIMLEY-HORN AND ASSOCIATES, INC. 111 W. JACKSON, SUITE 1320 CHICAGO, IL 60604 (630) 487-3432 JAKE.PANTER@KIMLEY-HORN.COM

SITE DATA TABLE

SITE ADDRESS:	8213 GOLDEN LINK BLVD MACEDONIA, OH 44067
PARCEL ID NO:	33125B2
TOTAL SITE AREA:	1.943 AC
EXISTING BUILDING AREA:	84,637 SF
ZONING:	4,861 SF B-3: GENERAL BUSINESS DISTRICT

FEMA FIRM FLOOD ZONE INFORMATION

ZONE:	X (UNSHADED)
DEFINITION:	AREA OF MINIMAL FLOOD HAZARD.
FIRM PANEL NO:	39153C0034E
COMMUNITY:	CITY OF MACEDONIA
CID NO:	390750
EFFECTIVE DATE:	07/20/2009
REVISION DATE:	NONE



ALTA / NSPS Land and Title Survey

KIMLEY HORN

8213 Golden Link Blvd.
City of Macedonia
Summit County, State of Ohio

Revisions / Submissions

ID	Description	Date

Project Number: 762055
 Scale: 1"=20'
 Drawn By: ATW
 Checked By: SWC
 Date: February 15, 2023
 Issue:

Drawing Title:

Akron-Cleveland Road (S.R. 8) (190')

Aurora Road (S.R. 82) (width varies)

Golden Link Blvd. (width varies)

MONUMENT LEGEND

- REBAR/IRON PIN FOUND
- CONCRETE MONUMENT FOUND

LEGEND

- EXISTING PROPERTY BOUNDARY
- EXISTING PROPERTY LINE
- EXISTING RIGHT-OF-WAY
- EXISTING EASEMENT
- EXISTING CENTERLINE
- EXISTING PAVEMENT MARKING
- EXISTING PAVEMENT
- EXISTING FACE OF CURB
- EXISTING BACK OF CURB
- EXISTING DRIVE
- EXISTING SIDEWALK
- EXISTING SANITARY SEWER LINE
- EXISTING STORM SEWER LINE
- EXISTING WATERLINE
- EXISTING GAS LINE
- EXISTING OVERHEAD LINE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING FIBER OPTIC
- EXISTING UNDERGROUND TELEPHONE
- EXISTING FENCE
- EXISTING WATER
- EXISTING METLAND
- EXISTING DITCH
- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- EXISTING STORM INLET/CATCH BASIN
- EXISTING STORM MANHOLE
- EXISTING STORM CURB INLET
- EXISTING HEADWALL
- EXISTING SANITARY MANHOLE
- EXISTING SIGN
- EXISTING ELECTRIC TRANSFORMER
- EXISTING ELECTRIC BOX
- EXISTING UTILITY POLE
- EXISTING LIGHT POLE
- EXISTING MAILBOX
- EXISTING GAS METER
- EXISTING GREASE MANHOLE
- EXISTING STRUCTURE NUMBER
- EXISTING GUY WIRE
- EXISTING TELEPHONE BOX
- EXISTING CLEANOUT
- EXISTING BOLLARD
- EXISTING FLAG
- EXISTING WATER METER
- EXISTING DECIDUOUS TREE
- EXISTING BUSH

Storm Structures		Sanitary Structures	
5066 - STORM MANHOLE RIM = 974.74 NO VISIBLE FLOWLINES TO BOTTOM = 953.2' WATER LEVEL = 964.2'	5492 - STORM MANHOLE RIM = 972.83 FL 24" HDPE SW = 965.7' FL 24" HDPE SSE = 964.3' FL 24" HDPE NE = 964.0'	5805 - SANITARY MANHOLE RIM = 972.95' FL 6" PVC SE = 965.7'	6071 - SANITARY MANHOLE RIM = 972.69' NO DIP - COULD NOT ACCESS
5132 - STORM MANHOLE RIM = 974.54 TO BOTTOM = 961.4' WATER LEVEL = 962.0'	5621 - INLET BASIN RIM = 971.94 FL 12" HDPE N = 967.4' FL 12" HDPE SE = 967.3'	6123 - SANITARY MANHOLE RIM = 971.38' FL 60" RCP SE = 961.6' FL 60" RCP NW = 971.4'	6124 - SANITARY MANHOLE RIM = 971.68' FL 60" RCP E = 966.4' FL 60" RCP W = 971.7'
5161 - INLET BASIN RIM = 973.14 FL 12" HDPE SSW = 969.2'	5685 - INLET BASIN RIM = 972.02 FL 12" HDPE NW = 966.9' FL 12" HDPE S = 966.9' FL 12" HDPE E = 966.9'	5762 - INLET BASIN RIM = 971.58 CAR PARKED ON STRUCTURE TO BOTTOM = 966.2'	5971 - CATCH BASIN RIM = 971.96 NO VISIBLE FLOWLINES EROSION CONTROL INSTALLED ON STRUCTURE
5309 - STORM MANHOLE RIM = 974.38 FL 12" HDPE NNE = 964.1' FL 24" HDPE SSW = 964.1' FL 24" HDPE NE = 963.8' TOP WEIR N & S = 970.6'	5383 - INLET BASIN RIM = 971.29 FL 12" HDPE NE = 967.5' FL 12" HDPE W = 967.4'	6065 - CATCH BASIN RIM = 971.01 NO VISIBLE FLOWLINES EROSION CONTROL INSTALLED ON STRUCTURE	6123 - STORM MANHOLE RIM = 971.38 FL 60" RCP SE = 961.6' FL 60" RCP NW = 971.4'
5393 - STORM MANHOLE RIM = 972.27 FL 12" HDPE SW = 966.3' FL 24" HDPE E = 964.8' FL 24" HDPE NW = 964.5'	5491 - CATCH BASIN		

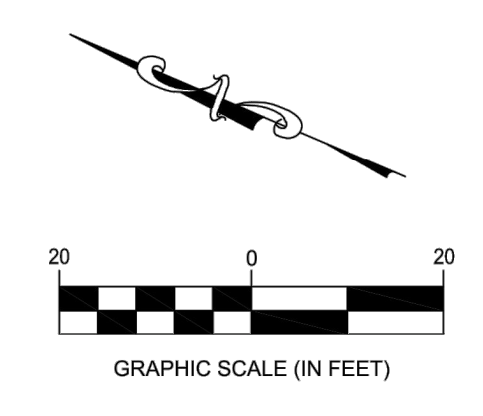
BENCHMARK

Vertical Datum NAVD88 derived from GPS Observations

BM "A": "x" set east of water vault northwest 58 feet from the intersection of Golden Link Blvd. and the entrance to Chick-Fil-A, and 25 feet northeast from the centerline of Golden Link Blvd. Elevation = 973.82'

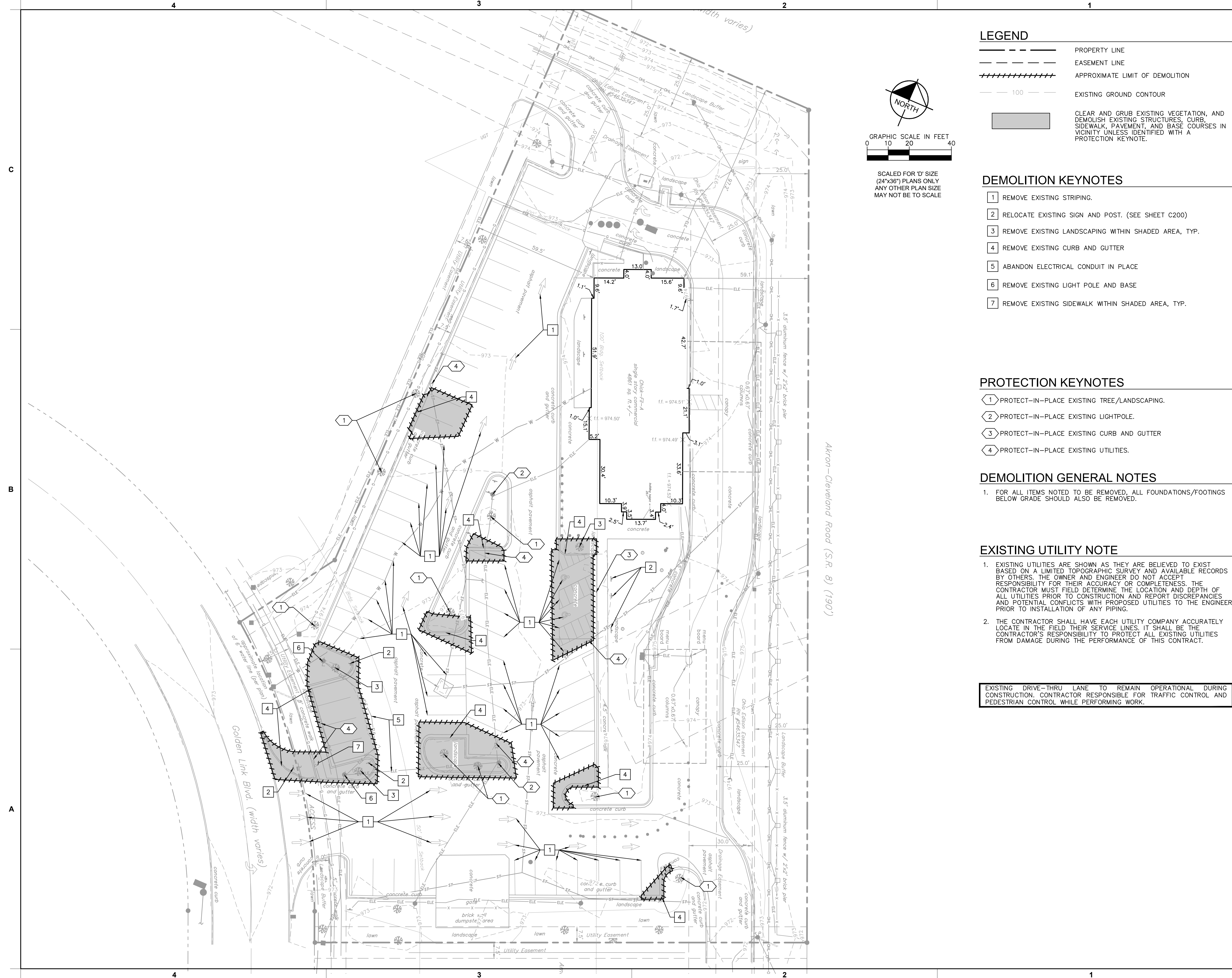
BM "B": Mag set in a power pole that is 137 feet west from the intersection of Aurora Rd. and Akron-Cleveland Rd., and 72 feet south of Aurora Rd. Elevation = 976.03'

BM "C": Mag set in a power telephone pole that is 407 feet southeast from the intersection of Aurora Rd. and Akron-Cleveland Rd., and 120 feet southwest of Akron-Cleveland Rd. Elevation = 973.06'



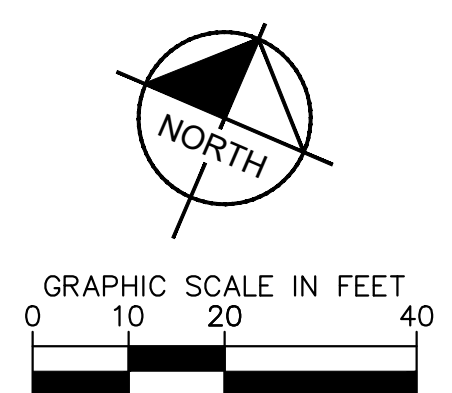
BASIS OF BEARINGS
 NAD 83 (2011), OHIO NORTH
 Derived from GPS Observations

\\kimley-horn\mym\CHICKS_LDEV\Chick-Fil-A\2023\FROM OTHER SURVEYS\KIM - Chick-Fil-A - Macedonia_OH_10 client 20230216.dwg - 3/15/2023 - Park, Alex



LEGEND

- PROPERTY LINE
- - - EASEMENT LINE
- ////// APPROXIMATE LIMIT OF DEMOLITION
- - - 100 - - - EXISTING GROUND CONTOUR
- █ CLEAR AND GRUB EXISTING VEGETATION, AND DEMOLISH EXISTING STRUCTURES, CURB, SIDEWALK, PAVEMENT, AND BASE COURSES IN VICINITY UNLESS IDENTIFIED WITH A PROTECTION KEYNOTE.



SCALED FOR 'D' SIZE (24"x36") PLANS ONLY
ANY OTHER PLAN SIZE MAY NOT BE TO SCALE

DEMOLITION KEYNOTES

- 1 REMOVE EXISTING STRIPING.
- 2 RELOCATE EXISTING SIGN AND POST. (SEE SHEET C200)
- 3 REMOVE EXISTING LANDSCAPING WITHIN SHADED AREA, TYP.
- 4 REMOVE EXISTING CURB AND GUTTER
- 5 ABANDON ELECTRICAL CONDUIT IN PLACE
- 6 REMOVE EXISTING LIGHT POLE AND BASE
- 7 REMOVE EXISTING SIDEWALK WITHIN SHADED AREA, TYP.

PROTECTION KEYNOTES

- 1 PROTECT-IN-PLACE EXISTING TREE/LANDSCAPING.
- 2 PROTECT-IN-PLACE EXISTING LIGHTPOLE.
- 3 PROTECT-IN-PLACE EXISTING CURB AND GUTTER
- 4 PROTECT-IN-PLACE EXISTING UTILITIES.

DEMOLITION GENERAL NOTES

1. FOR ALL ITEMS NOTED TO BE REMOVED, ALL FOUNDATIONS/FOOTINGS BELOW GRADE SHOULD ALSO BE REMOVED.

EXISTING UTILITY NOTE

1. EXISTING UTILITIES ARE SHOWN AS THEY ARE BELIEVED TO EXIST BASED ON A LIMITED TOPOGRAPHIC SURVEY AND AVAILABLE RECORDS BY OTHERS. THE OWNER AND ENGINEER DO NOT ACCEPT RESPONSIBILITY FOR THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR MUST FIELD DETERMINE THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION AND REPORT DISCREPANCIES AND POTENTIAL CONFLICTS WITH PROPOSED UTILITIES TO THE ENGINEER PRIOR TO INSTALLATION OF ANY PIPING.
2. THE CONTRACTOR SHALL HAVE EACH UTILITY COMPANY ACCURATELY LOCATE IN THE FIELD THEIR SERVICE LINES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT ALL EXISTING UTILITIES FROM DAMAGE DURING THE PERFORMANCE OF THIS CONTRACT.

EXISTING DRIVE-THRU LANE TO REMAIN OPERATIONAL DURING CONSTRUCTION, CONTRACTOR RESPONSIBLE FOR TRAFFIC CONTROL AND PEDESTRIAN CONTROL WHILE PERFORMING WORK.



Chick-fil-A

Chick-fil-A
5200 Buffington Road
Atlanta, Georgia 30349-2998

Kimley»Horn

© 2023 KIMLEY-HORN AND ASSOCIATES, INC.
111 WEST JACKSON BLVD., SUITE 11320, CHICAGO, IL 60604
PHONE: 312-726-9445
WWW.KIMLEY-HORN.COM

CHICK-FIL-A
MACEDONIA FSU
8213 GOLDEN LINK BLVD
MACEDONIA, OH 44067

FSR#2216

REVISION SCHEDULE

NO.	DATE	DESCRIPTION

CONSULTANT PROJECT # 268214000
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DATE 02/23/2023
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SHEET
DEMOLITION PLAN

SHEET NUMBER
C100

SITE IMPROVEMENT LEGEND

- PROPERTY LINE
- EASEMENT LINE
- +++++ APPROXIMATE PROJECT AREA LIMITS
- ▽▽▽ REMOVE AND REPLACE EXISTING CONCRETE PAVEMENT [DETAIL 1/C402]
- REMOVE EXISTING ASPHALT PAVEMENT

EROSION CONTROL SCHEDULE AND SEQUENCING:

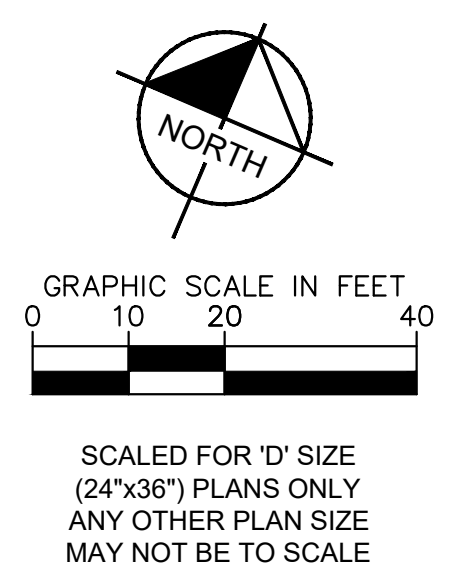
- I. ROUGH GRADING CONSTRUCTION ENTRANCE/EXIT, SILT FENCE PROTECTION, CONCRETE WASHOUT AREA AND TREE PROTECTION SHALL BE INSTALLED PRIOR TO THE INITIATION OF ROUGH GRADING, AS NEEDED. TEMPORARY EROSION CONTROL MEASURES TO BE INSTALLED UPON COMPLETION OF ROUGH GRADING AND AS NECESSARY THROUGHOUT CONSTRUCTION.
- II. UTILITY INSTALLATION ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING UTILITY INSTALLATION. STORM STRUCTURE INLET PROTECTION SHALL BE INSTALLED AS STORM DRAINAGE SYSTEM IS CONSTRUCTED.
- III. PAVING ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING PAVING AND THROUGHOUT THE REMAINDER OF THE PROJECT.
- IV. FINAL GRADING/SOIL STABILIZATION/LANDSCAPING ALL TEMPORARY EROSION CONTROL MEASURES TO BE REMOVED AT THE CONCLUSION OF THE PROJECT AS DIRECTED BY THE LOCAL MUNICIPALITY.

EROSION CONTROL LEGEND

- TS TEMPORARY SEEDING (SEE EROSION CONTROL DETAILS)
- CE CONSTRUCTION ENTRANCE
- DC DUST CONTROL
- FS FILTER SOCK SEDIMENT CONTROL (SEE EROSION CONTROL DETAILS)
- IP INLET PROTECTION (SEE EROSION CONTROL DETAILS)
- CW CONCRETE WASHOUT (SEE EROSION CONTROL DETAILS) (TO BE DETERMINED BY CONTRACTOR)
- LD LIMITS OF DISTURBANCE
- XXX--- EXISTING CONTOURS
- XXX--- PROPOSED CONTOURS

EROSION CONTROL NOTES

1. CONSTRUCTION ENTRANCE SHALL BE LOCATED SO AS TO PROVIDE THE LEAST AMOUNT OF DISTURBANCE TO THE FLOW OF TRAFFIC IN AND OUT OF THE SITE. ADDITIONALLY, CONSTRUCTION ENTRANCE SHALL BE LOCATED TO COINCIDE WITH THE PHASING OF THE PAVEMENT REPLACEMENT.
2. POST CONSTRUCTION STORM WATER POLLUTION CONTROL MEASURES INCLUDE STABILIZATION BY PERMANENT PAVING, DRAINAGE SYSTEM STRUCTURE, OR LANDSCAPING.
3. TEMPORARY AND PERMANENT STABILIZATION PRACTICES AND BMP'S SHALL BE INSTALLED AT THE EARLIEST POSSIBLE TIME DURING THE CONSTRUCTION SEQUENCE. AS AN EXAMPLE, PERIMETER SILT FENCE SHALL BE INSTALLED BEFORE COMMENCEMENT OF ANY GRADING ACTIVITIES. OTHER BMP'S SHALL BE INSTALLED AS SOON AS PRACTICABLE AND SHALL BE MAINTAINED UNTIL FINAL SITE STABILIZATION IS ATTAINED. CONTRACTOR SHALL ALSO REFERENCE CIVIL AND LANDSCAPE PLANS SINCE PERMANENT STABILIZATION IS PROVIDED BY LANDSCAPING, THE BUILDING(S), AND SITE PAVING.
4. BMP'S HAVE BEEN LOCATED AS INDICATED ON THIS PLAN IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES IN ORDER TO MINIMIZE SEDIMENT TRANSFER. FOR EXAMPLE: SILT FENCES LOCATED AT TOE OF SLOPE AND INLET PROTECTION FOR INLETS RECEIVING SEDIMENT FROM SITE RUN-OFF.
5. THE PLACEMENT OF EROSION/SEDIMENTATION CONTROLS SHALL BE IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENTATION CONTROL PLAN.
6. ANY MAJOR VARIATION IN MATERIALS OR LOCATIONS OF CONTROLS OR FENCES FROM THOSE SHOWN ON THE APPROVED PLANS WILL REQUIRE A REVISION AND MUST BE APPROVED BY THE REVIEWING ENGINEER, ENVIRONMENTAL SPECIALIST, OR ARBORIST AS APPROPRIATE. MAJOR REVISIONS MUST BE APPROVED BY THE PLANNING AND DEVELOPMENT DEPARTMENT AND THE DRAINAGE UTILITY DEPARTMENT. MINOR CHANGES OR ADDITIONAL CONTROL MEASURES TO BE MADE AS FIELD REVISIONS TO THE EROSION AND SEDIMENTATION CONTROL PLAN MAY BE REQUIRED BY THE ENVIRONMENTAL INSPECTOR DURING THE COURSE OF CONSTRUCTION TO CORRECT CONTROL INADEQUACIES AT NO ADDITIONAL COST TO THE OWNER.
7. CONTRACTOR SHALL PLACE EROSION CONTROL BLANKET (NORTH AMERICAN GREEN S150BN OR APPROVED EQUAL) ON ALL SITE AREAS WITH SLOPES GREATER THAN 4:1, AND IN THE BOTTOM AND SIDE SLOPES OF ALL SWALES.
8. PRIOR TO FINAL ACCEPTANCE, HAUL ROADS AND WATERWAY CROSSINGS CONSTRUCTED FOR TEMPORARY CONTRACTOR ACCESS MUST BE REMOVED, ACCUMULATED SEDIMENT REMOVED FROM THE WATERWAY AND THE AREA RESTORED TO THE ORIGINAL GRADE AND REVEGETATED. ALL LAND CLEARING SHALL BE DISPOSED OF IN APPROVED SPOIL DISPOSAL SITES.
9. PERMANENT, FINAL PLANT COVERING OR STRUCTURES SHALL BE INSTALLED PRIOR TO FINAL ACCEPTANCE.
10. ALL CONTROL DEVICES THAT FUNCTION SIMILARLY TO SILT FENCE OR FIBER ROLLS MUST BE REPAIRED, REPLACED OR SUPPLEMENTED WITH EFFECTIVE CONTROLS WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES ONE-THIRD THE HEIGHT OF THE DEVICE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF THE RAINFALL EVENT OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
11. ALL SEDIMENT DELTAS AND DEPOSITS MUST BE REMOVED FROM SURFACE WATERS, DRAINAGE WAYS, CATCH BASINS AND OTHER DRAINAGE SYSTEMS. ALL AREAS WHERE SEDIMENT REMOVAL RESULTED IN EXPOSED SOIL MUST BE RESTABILIZED. THE REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS AFTER THE RAINFALL EVENT UNLESS PRECLUDED BY LEGAL, REGULATORY OR PHYSICAL ACCESS CONSTRAINTS. ALL REASONABLE EFFORTS MUST BE USED TO OBTAIN ACCESS, ONCE ACCESS IS OBTAINED, REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS LATER. CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL APPROPRIATE AUTHORITIES AND RECEIVING THE APPLICABLE PERMITS PRIOR TO CONDUCTING ANY WORK.
12. ACCUMULATIONS OF TRACKED AND DEPOSITED SEDIMENT MUST BE REMOVED FROM OFF-SITE PAVED SURFACES WITHIN 24 HOURS OR SOONER IF REQUIRED. SEDIMENT TRACKING MUST BE MINIMIZED BY THE APPROPRIATE MANAGEMENT PRACTICE, LIKE A DEDICATED SITE EXIT WITH AN AGGREGATE SURFACE OR DESIGNATED OFF-SITE PARKING AREA. CONTRACTOR IS RESPONSIBLE FOR STREET SWEEPING AND/OR SCRAPING IF YOUR PRACTICES ARE NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED FROM THE SITE.
13. SURFACE WATERS, DRAINAGE DITCHES AND CONVEYANCE SYSTEMS MUST BE INSPECTED FOR SEDIMENT DEPOSITS.
14. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES AS INDICATED ON THIS SHEET IN ACCORDANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) PREPARED BY KIMLEY-HORN AND ASSOCIATES, INC. THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING THE PROVISIONS INDICATED IN THE SWPPP, INCLUDING EROSION CONTROL MEASURES AND INSPECTION FREQUENCY, AS REQUIRED BY THE IEPA NPDES PHASE II PERMIT PROGRAM REQUIREMENTS.
15. PUMPING SEDIMENT LADEN WATER INTO ANY STORMWATER FACILITY THAT IS NOT DESIGNATED TO BE A SEDIMENT TRAP, DRAINAGEWAY, OR OFFSITE AREA EITHER DIRECTLY OR INDIRECTLY WITHOUT FILTRATION IS PROHIBITED.
16. SOIL STOCKPILES SHALL NOT BE LOCATED IN A DRAINAGEWAY, FLOOD PLAIN AREA OR A DESIGNATED BUFFER, UNLESS OTHERWISE APPROVED, UNDER SPECIFIC CONDITIONS TO BE ESTABLISHED BY THE DIRECTOR OR ADMINISTRATOR.
17. STOCKPILES TO REMAIN IN PLACE FOR MORE THAN THREE DAYS SHALL BE PROVIDED WITH SESC MEASURES. MATERIAL IS TO BE HAULED OFF IMMEDIATELY AND LEGALLY IF NO STOCKPILE IS TO REMAIN IN PLACE.
18. ALL TEMPORARY SESC MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED. TRAPPED SEDIMENT AND OTHER DISTURBED SOILS RESULTING FROM TEMPORARY MEASURES SHALL BE PROPERLY DISPOSED OF PRIOR TO PERMANENT STABILIZATION.
19. WATER REMOVED FROM TRAPS, BASINS, AND OTHER WATER HOLDING DEPRESSIONS OR EXCAVATIONS MUST FIRST PASS THROUGH A SEDIMENT CONTROL AND/OR FILTRATION DEVICE WHEN DEWATERING DEVICES ARE USED, DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION.
20. SITE STABILIZATION REQUIREMENTS ARE AS FOLLOWS:
 - 20.1. WHERE THE INITIATION OF STABILIZATION MEASURE BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES ON A PORTION OF THE SITE IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURE SHALL BE INITIATED AS SOON AS PRACTICABLE.
 - 20.2. WHERE CONSTRUCTION ACTIVITY WILL RESUME ON A PORTION OF THE SITE WITHIN 14 DAYS FROM WHEN ACTIVITIES CEASED, (E.G. THE TOTAL TIME PERIOD THAT CONSTRUCTION ACTIVITY IS TEMPORARILY CEASED IS LESS THAN 14 DAYS) THEN STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY CEASED.



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15 May 2023



Chick-fil-A

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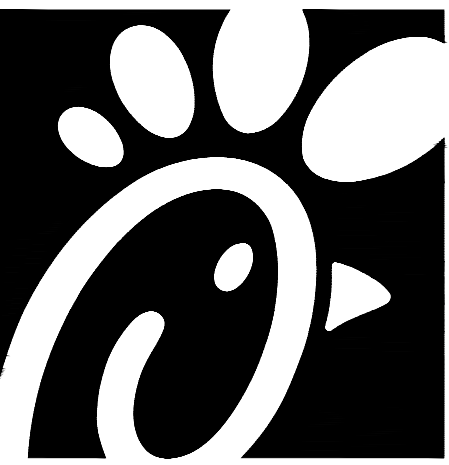
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SHEET
EROSION CONTROL PLAN

SHEET NUMBER
C101

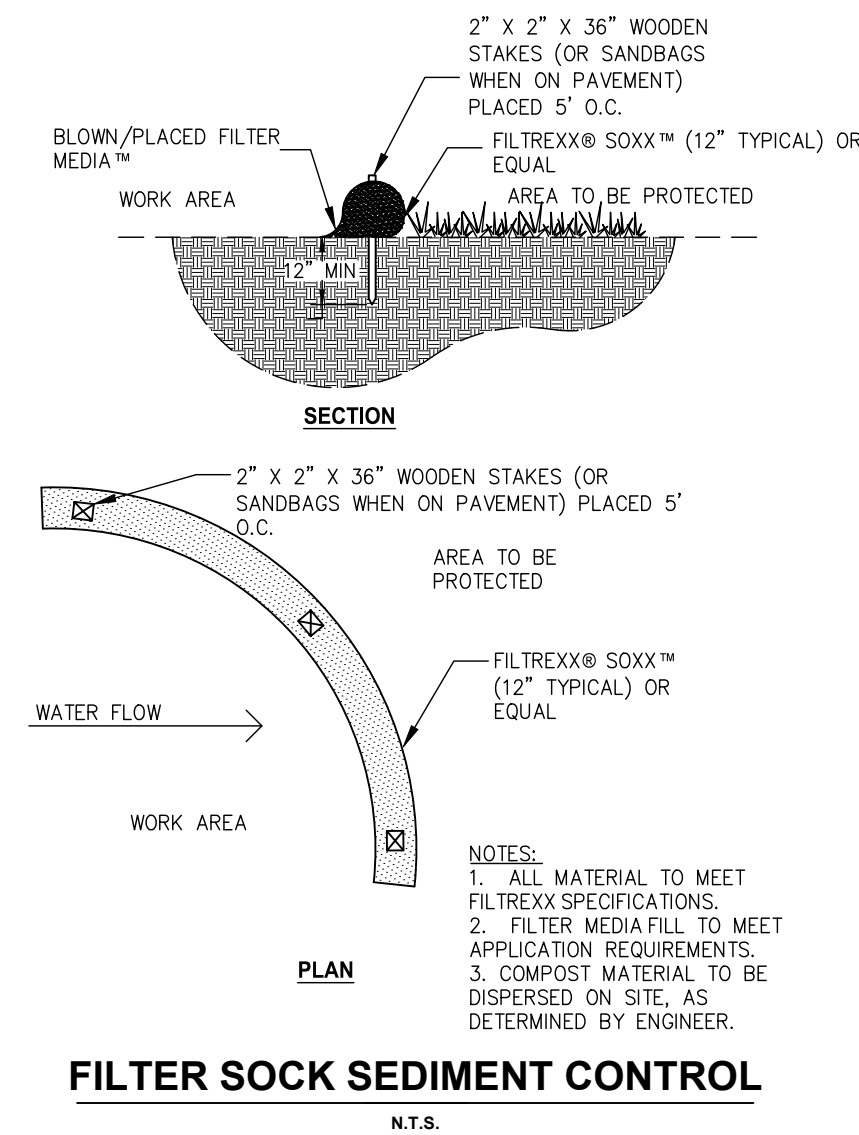


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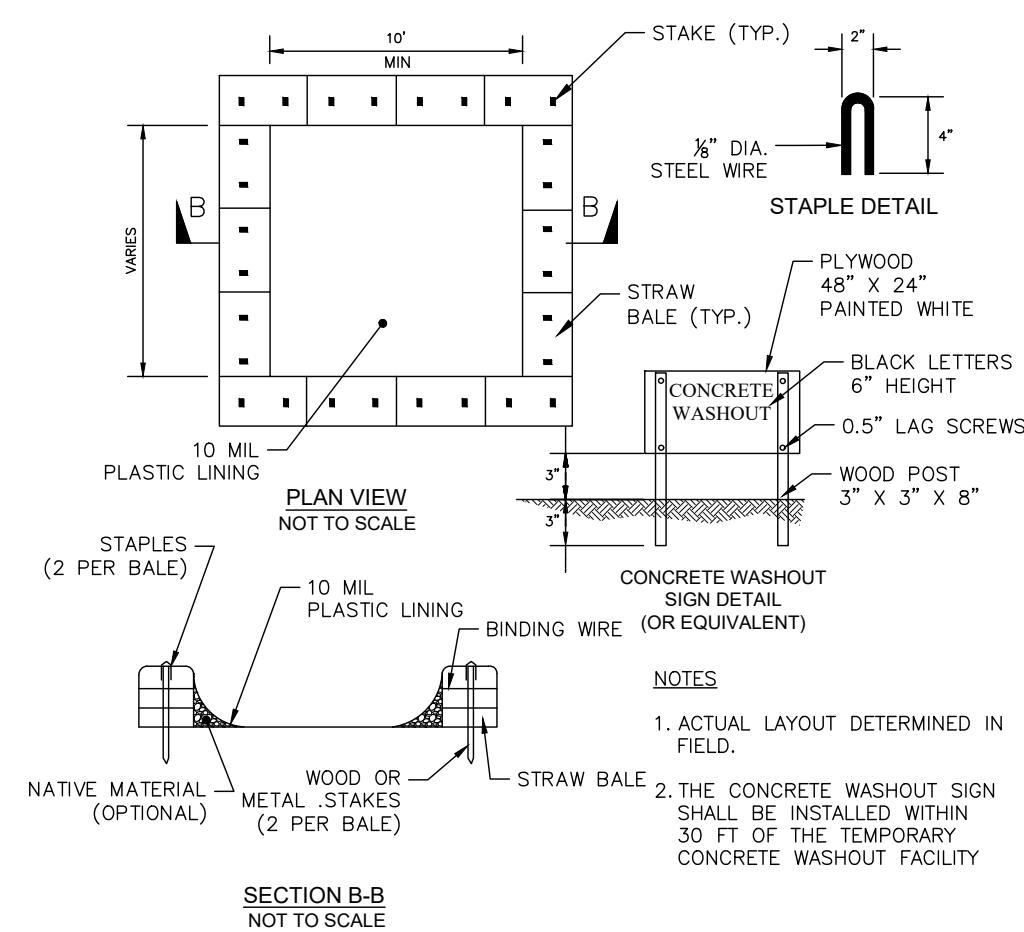
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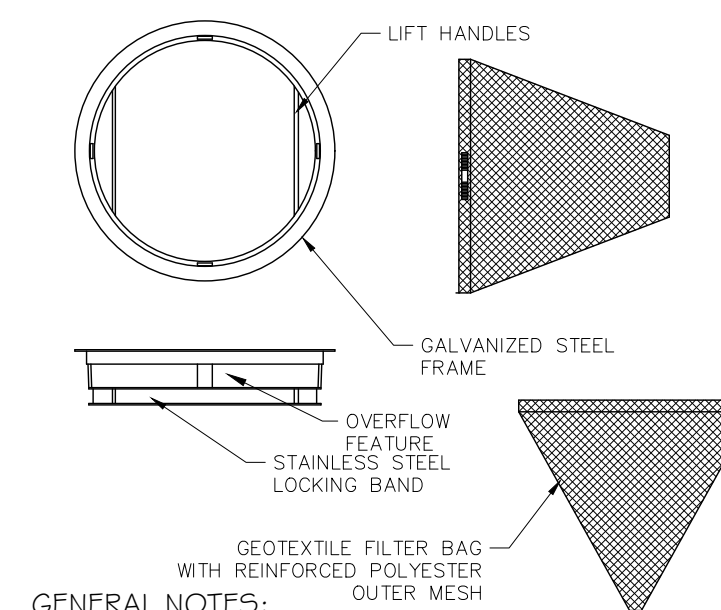
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FILTER SOCK SEDIMENT CONTROL



CONCRETE WASHOUT



GENERAL NOTES:

ALL PRODUCTS MANUFACTURED BY INLET AND PIPE PROTECTION, INC OR APPROVED EQUAL.

NOTE: INLET FILTERS ARE SLIGHTLY SMALLER THAN THE INLET GRATE SIZES. WHEN IDENTIFYING OR SPECIFYING FILTERS/CASTINGS PLEASE REFER TO THE DIAMETER "D" OR WIDTH "W" AND HEIGHT "H" OF FILTER FRAMES OR CASTING GRATES. YOU MAY ALSO REFER TO OUR CASTING CROSS REFERENCE GUIDE FOR IDOT STANDARDS.

IDOT TYPE 1 ROUND INLET FILTER DEPICTED

NOTE: ROUND AND SQUARE INLET FILTERS AVAILABLE FOR MOST NEENAH AND EAST JORDAN BEEHIVE, ROLL CURB AND CURB BOX FRAME TYPES

ALL IPP INLET FILTERS TO CONFORM TO IDOT SPECIFICATIONS AS OUTLINED IN ARTICLE 1081.15 OF IDOT'S STANDARD SPECIFICATIONS GUIDE.

INLET PROTECTION

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY	JUNE	JULY	AUG.	SEPT.	OCT.	NOV.	DEC.
PERMANENT SEEDING			A			*	*					
DORMANT SEEDING	B										B	
TEMPORARY SEEDING			C				D					
SODDING			E									
MULCHING	F		**									

- A KENTUCKY BLUEGRASS 90 LBS/ACRE MIXED WITH PERENNIAL RYEGRASS 30 LBS/ACRE
- B KENTUCKY BLUEGRASS 135 LBS/ACRE MIXED WITH PERENNIAL RYEGRASS 45 LBS/ACRE + 2 TONS STRAW MULCH/ACRE
- C SPRING OATS 100 LBS/ACRE
- D WHEAT OR CEREAL RYE 150 LBS/ACRE
- E SOD
- F STRAW MULCH 2 TONS/ACRE
- * WATERING NEEDED DURING JUNE AND JULY
- ** WATERING NEEDED FOR 2 TO 3 WEEKS AFTER APPLYING SOD

SEEDING CHART

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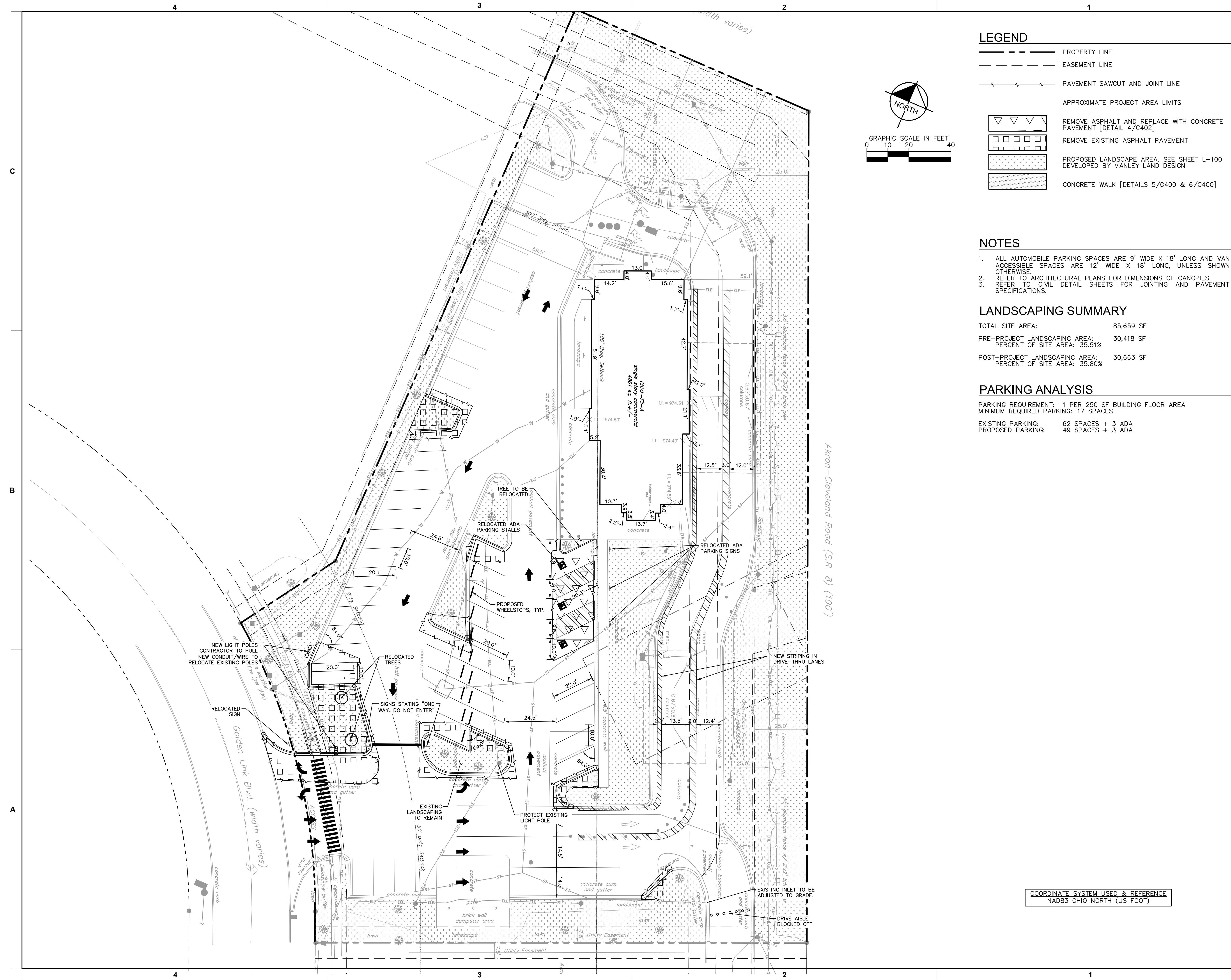
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SHEET
EROSION CONTROL DETAILS

SHEET NUMBER

C102

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17 May 2023



LEGEND

- PROPERTY LINE
- EASEMENT LINE
- PAVEMENT SAWCUT AND JOINT LINE
- APPROXIMATE PROJECT AREA LIMITS
- REMOVE ASPHALT AND REPLACE WITH CONCRETE PAVEMENT [DETAIL 4/C402]
- REMOVE EXISTING ASPHALT PAVEMENT
- PROPOSED LANDSCAPE AREA. SEE SHEET L-100 DEVELOPED BY MANLEY LAND DESIGN
- CONCRETE WALK [DETAILS 5/C400 & 6/C400]

- NOTES**
1. ALL AUTOMOBILE PARKING SPACES ARE 9' WIDE X 18' LONG AND VAN ACCESSIBLE SPACES ARE 12' WIDE X 18' LONG, UNLESS SHOWN OTHERWISE.
 2. REFER TO ARCHITECTURAL PLANS FOR DIMENSIONS OF CANOPIES.
 3. REFER TO CIVIL DETAIL SHEETS FOR JOINTING AND PAVEMENT SPECIFICATIONS.

LANDSCAPING SUMMARY

TOTAL SITE AREA:	85,659 SF
PRE-PROJECT LANDSCAPING AREA:	30,418 SF
PERCENT OF SITE AREA:	35.51%
POST-PROJECT LANDSCAPING AREA:	30,663 SF
PERCENT OF SITE AREA:	35.80%

PARKING ANALYSIS

PARKING REQUIREMENT: 1 PER 250 SF BUILDING FLOOR AREA
MINIMUM REQUIRED PARKING: 17 SPACES

EXISTING PARKING:	62 SPACES + 3 ADA
PROPOSED PARKING:	49 SPACES + 3 ADA



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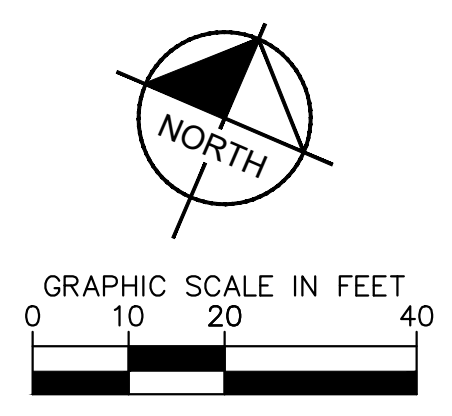
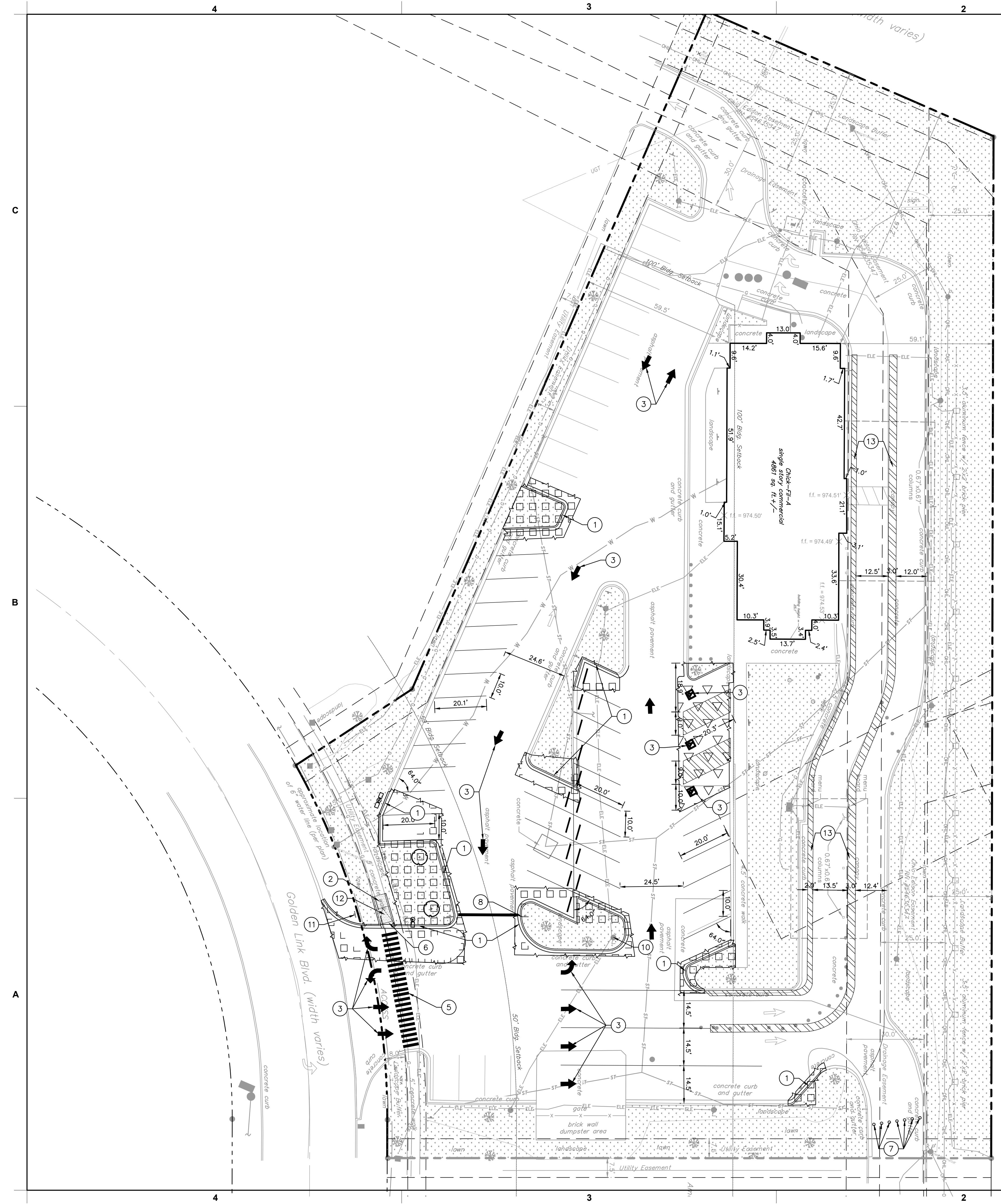
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SHEET
SITE AND LANDSCAPE PLAN

SHEET NUMBER
C200



LEGEND

	PROPERTY LINE
	EASEMENT LINE
	PAVEMENT SAWCUT AND JOINT LINE
	APPROXIMATE PROJECT AREA LIMITS
	REMOVE ASPHALT AND REPLACE WITH CONCRETE PAVEMENT [DETAIL 4/C402]
	REMOVE EXISTING ASPHALT PAVEMENT
	PROPOSED LANDSCAPE AREA. SEE SHEET L-100 DEVELOPED BY MANLEY LAND DESIGN
	CONCRETE WALK [DETAILS 5/C400 & 6/C400]

- NOTES**
- ALL AUTOMOBILE PARKING SPACES ARE 9' WIDE X 18' LONG AND VAN ACCESSIBLE SPACES ARE 12' WIDE X 18' LONG, UNLESS SHOWN OTHERWISE.
 - REFER TO ARCHITECTURAL PLANS FOR DIMENSIONS OF CANOPIES.
 - REFER TO CIVIL DETAIL SHEETS FOR JOINTING AND PAVEMENT SPECIFICATIONS.

LANDSCAPING SUMMARY

TOTAL SITE AREA:	85,659 SF
PRE-PROJECT LANDSCAPING AREA:	30,418 SF
PERCENT OF SITE AREA:	35.51%
POST-PROJECT LANDSCAPING AREA:	30,663 SF
PERCENT OF SITE AREA:	35.80%

- SITE PLAN KEYNOTES**
- CONSTRUCT CONCRETE CURB AND GUTTER PER DETAIL 6, SHEET C400.
 - CONSTRUCT CONCRETE WALK PER DETAIL 5, SHEET C400.
 - PAINT STRIPING PER DETAIL 1, SHEET C400.
 - PAINT STRIPING PER DETAIL 2, SHEET C400.
 - PAINT CROSSWALK MARKINGS PER DETAIL 5, SHEET C401.
 - INSTALL DETECTABLE WARNING PER DETAIL 8, SHEET C400.
 - INSTALL 6" YELLOW PERMANENT BOLLARD PER DETAIL 4, SHEET C401.
 - INSTALL "DO NOT ENTER" SIGN AND POST PER DETAIL 4, SHEET C400
 - INSTALL "PEDESTRIAN CROSSWALK" SIGN AND POST PER DETAIL 4, SHEET C400
 - EXISTING LIGHT POLE TO REMAIN. NO CHANGE TO EXISTING PHOTOMETRICS*
* FULL SHIELDING BELOW THE LOWEST HORIZONTAL PLANE OF THE LIGHT-EMITTING PART OF ALL LIGHTING FIXTURES OVER 1000
 - RELOCATED STOP SIGN
 - ACCESSIBLE RAMP PER DETAIL 3, SHEET C402
 - PAINT STRIPING PER DETAIL 1, SHEET C402.

COORDINATE SYSTEM USED & REFERENCE
NAD83 OHIO NORTH (US FOOT)



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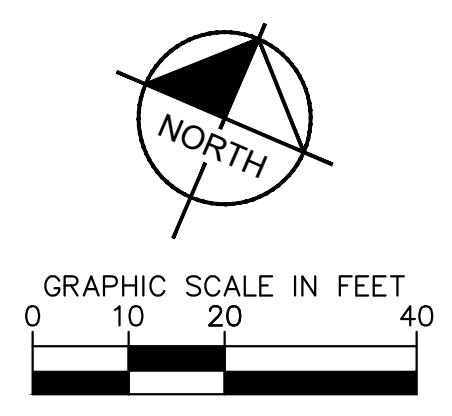
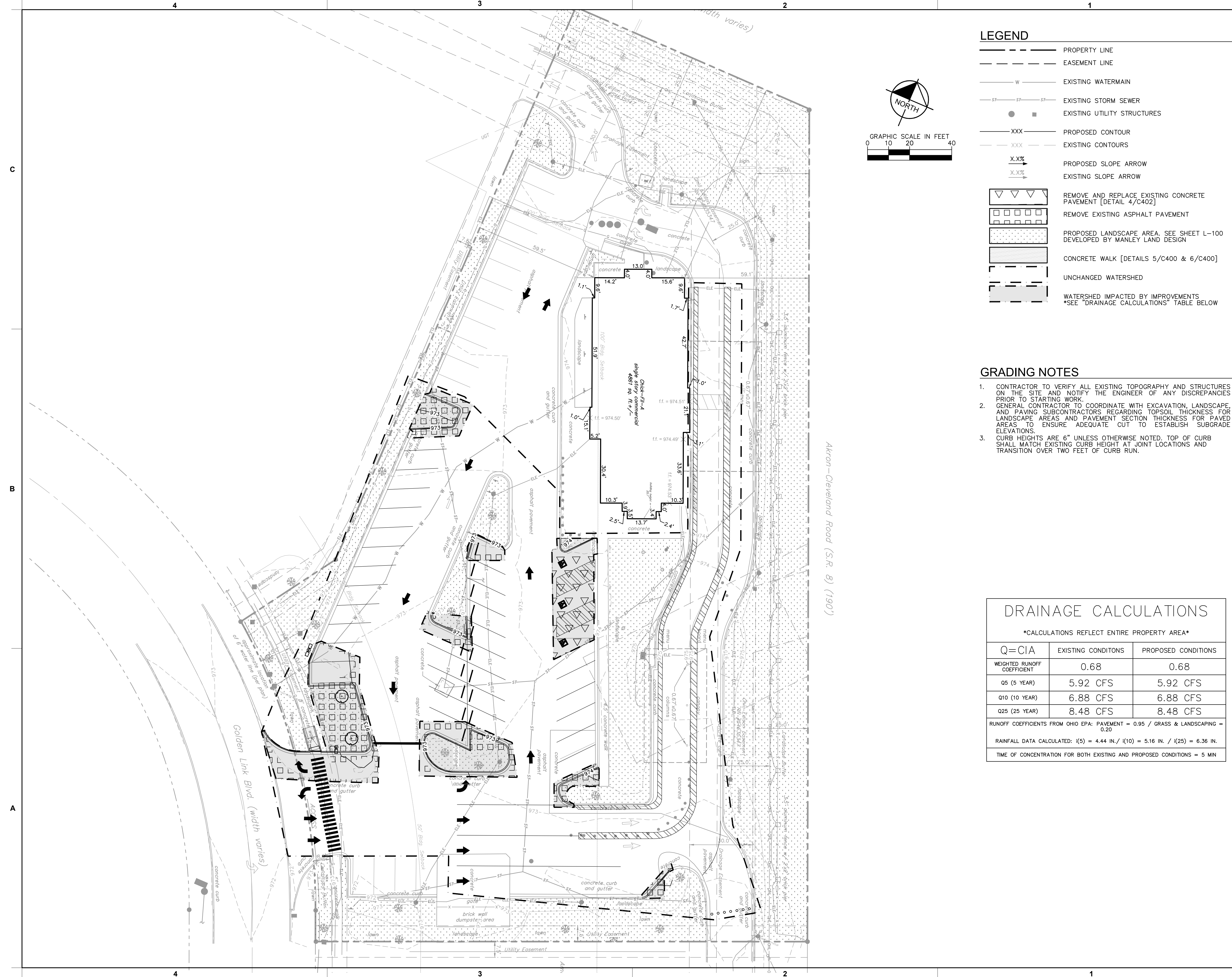
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SHEET
DETAILED SITE PLAN

SHEET NUMBER
C201

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17 May 2023



LEGEND

- — — — — PROPERTY LINE
- - - - - EASEMENT LINE
- W — — — — — EXISTING WATERMAIN
- ST — ST — ST — EXISTING STORM SEWER
- ■ EXISTING UTILITY STRUCTURES
- XXX — — — — PROPOSED CONTOUR
- - - - - XXX - - - - - EXISTING CONTOURS
- X.X% → PROPOSED SLOPE ARROW
- X.X% → EXISTING SLOPE ARROW

- REMOVE AND REPLACE EXISTING CONCRETE PAVEMENT [DETAIL 4/C402]
- REMOVE EXISTING ASPHALT PAVEMENT
- PROPOSED LANDSCAPE AREA. SEE SHEET L-100 DEVELOPED BY MANLEY LAND DESIGN
- CONCRETE WALK [DETAILS 5/C400 & 6/C400]
- UNCHANGED WATERSHED
- WATERSHED IMPACTED BY IMPROVEMENTS
*SEE "DRAINAGE CALCULATIONS" TABLE BELOW

- GRADING NOTES**
- CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES ON THE SITE AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
 - GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE, AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO ENSURE ADEQUATE CUT TO ESTABLISH SUBGRADE ELEVATIONS.
 - CURB HEIGHTS ARE 6" UNLESS OTHERWISE NOTED. TOP OF CURB SHALL MATCH EXISTING CURB HEIGHT AT JOINT LOCATIONS AND TRANSITION OVER TWO FEET OF CURB RUN.

DRAINAGE CALCULATIONS
CALCULATIONS REFLECT ENTIRE PROPERTY AREA

Q=CIA	EXISTING CONDITONS	PROPOSED CONDITIONS
WEIGHTED RUNOFF COEFFICIENT	0.68	0.68
Q5 (5 YEAR)	5.92 CFS	5.92 CFS
Q10 (10 YEAR)	6.88 CFS	6.88 CFS
Q25 (25 YEAR)	8.48 CFS	8.48 CFS

RUNOFF COEFFICIENTS FROM OHIO EPA: PAVEMENT = 0.95 / GRASS & LANDSCAPING = 0.20
 RAINFALL DATA CALCULATED: i(5) = 4.44 IN. / i(10) = 5.16 IN. / i(25) = 6.36 IN.
 TIME OF CONCENTRATION FOR BOTH EXISTING AND PROPOSED CONDITIONS = 5 MIN

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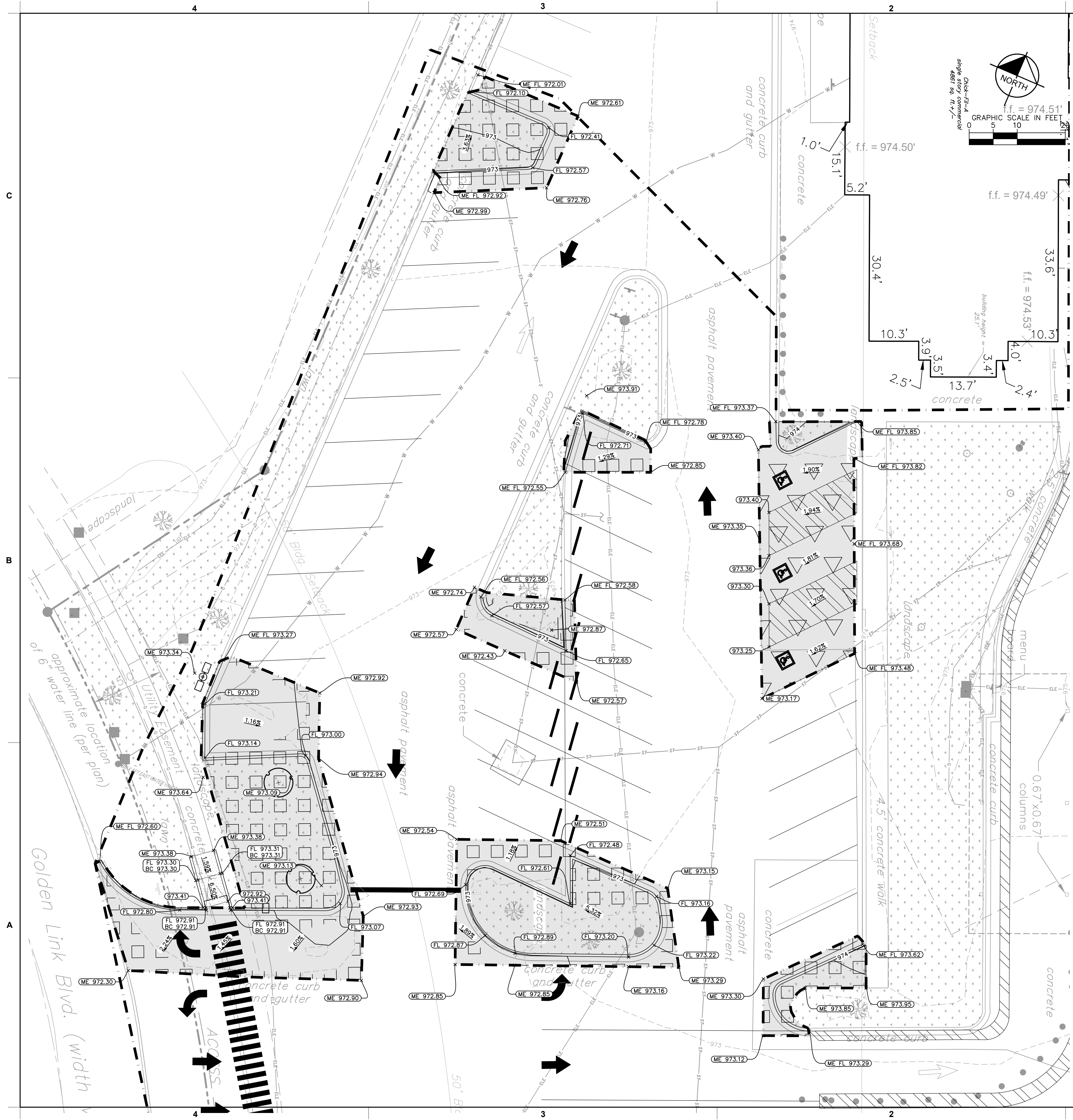
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SHEET DRAINAGE AND GRADING PLAN
 SHEET NUMBER **C300**

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17 May 2023



LEGEND

- PROPERTY LINE
- - - EASEMENT LINE
- W EXISTING WATERMAIN
- ST EXISTING STORM SEWER
- EXISTING UTILITY STRUCTURES
- PROPOSED CONTOUR
- XXX EXISTING CONTOURS
- X.X% PROPOSED SLOPE ARROW
- X.X% EXISTING SLOPE ARROW
- [Pattern] REMOVE AND REPLACE EXISTING CONCRETE PAVEMENT [DETAIL 4/C402]
- [Pattern] REMOVE EXISTING ASPHALT PAVEMENT
- [Pattern] PROPOSED LANDSCAPE AREA. SEE SHEET L-100 DEVELOPED BY MANLEY LAND DESIGN
- [Pattern] CONCRETE WALK [DETAILS 5/C400 & 6/C400]
- [Pattern] UNCHANGED WATERSHED
- [Pattern] WATERSHED IMPACTED BY IMPROVEMENTS
*SEE "DRAINAGE CALCULATIONS" TABLE BELOW

- GRADING NOTES**
- CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES ON THE SITE AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
 - GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE, AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO ENSURE ADEQUATE CUT TO ESTABLISH SUBGRADE ELEVATIONS.
 - CURB HEIGHTS ARE 6" UNLESS OTHERWISE NOTED. TOP OF CURB SHALL MATCH EXISTING CURB HEIGHT AT JOINT LOCATIONS AND TRANSITION OVER TWO FEET OF CURB RUN.



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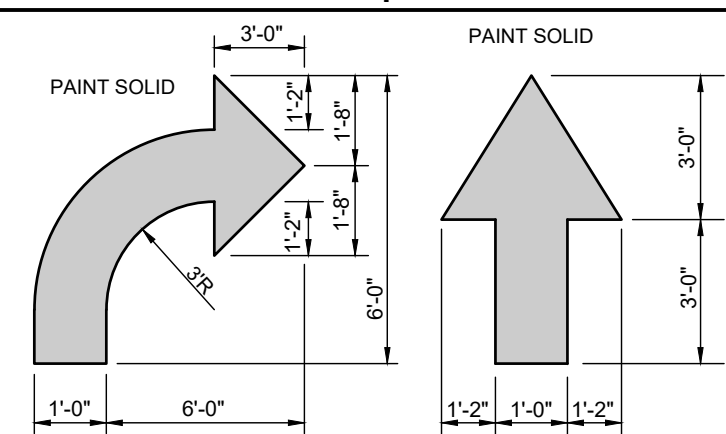
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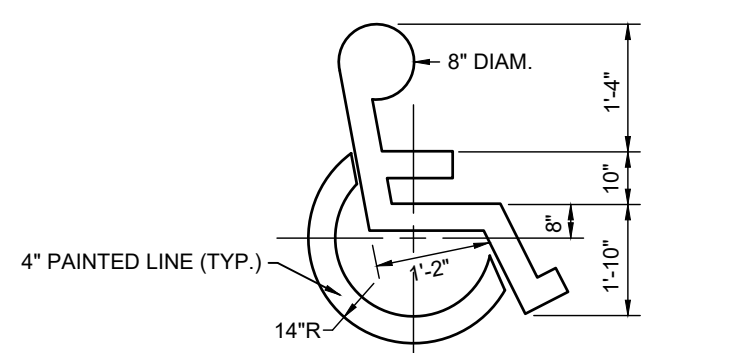
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SHEET NUMBER



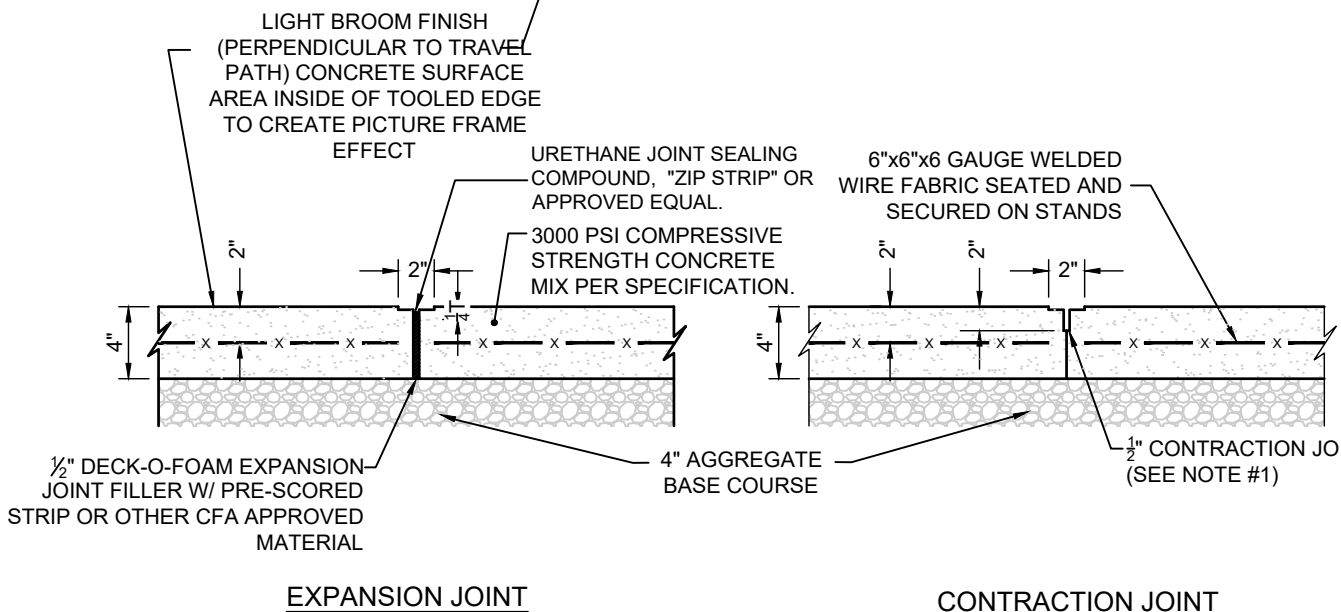
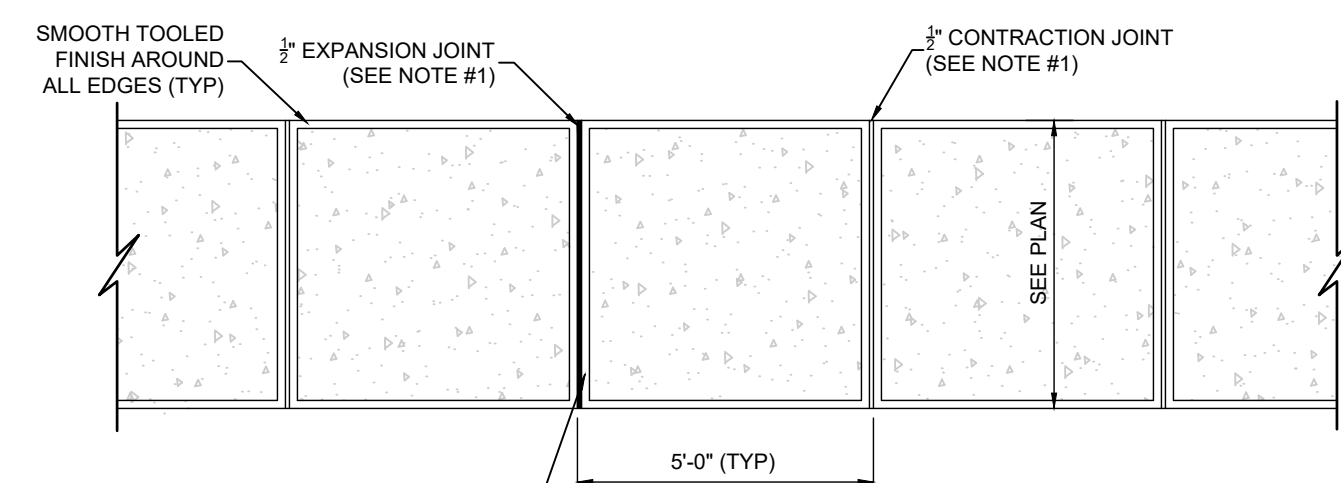
DIRECTIONAL ARROW
NOT TO SCALE



ACCESSIBILITY SYMBOL
NOT TO SCALE

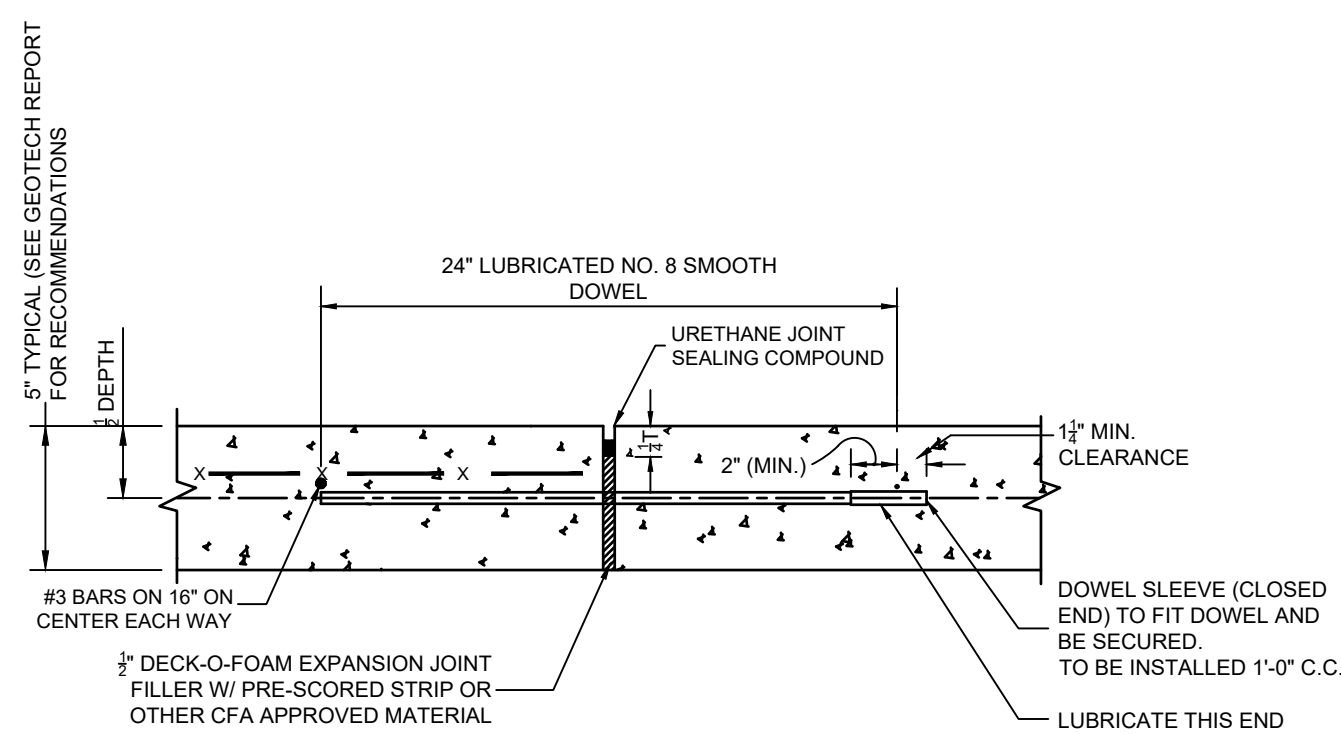
- NOTES:**
- GENERAL CONTRACTOR SHALL REFER TO PARKING LOT STRIPING SPECIFICATIONS. SEE DETAIL.
 - PAVEMENT MARKINGS SHALL BE APPLIED ACCORDING TO REQUIREMENTS AS OUTLINED IN SECTION 3B OF THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - CONTRACTOR SHALL USE WHITE REFLECTIVE PAINT ON ASPHALT & YELLOW REFLECTIVE PAINT ON CONCRETE, UNLESS UPON VERIFICATION BY THE GENERAL CONTRACTOR IT IS DETERMINED THAT LOCAL, STATE, OR ADA CODES DIFFER, IN WHICH CASE THESE CODES SHALL GOVERN.

1 PAVEMENT MARKINGS - 1
NOT TO SCALE



- NOTES:**
- JOINTS AT 5'-0" O.C. TOOLED 1/2" WIDE, 1" DEEP OR MAX. 3" DEEP WHICHEVER IS GREATER. EXPANSION JOINTS AT 20' MAX. & ALL P.C.s. UNLESS APPROVED OR INDICATED OTHERWISE ON PLAN VIEW JOINT PATTERN.

5 CONCRETE SIDEWALK
NOT TO SCALE



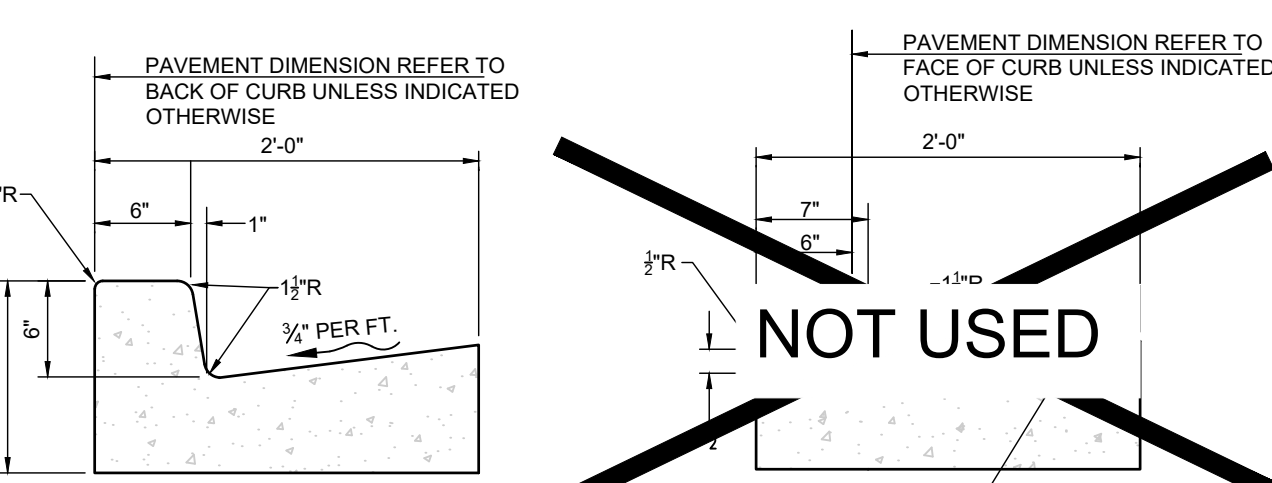
- NOTES:**
- NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 5 INCH AND 6 INCH PAVEMENT THICKNESS.
 - LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTOR'S OPTION.
 - DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL EQUIPMENT.
 - DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO WET CONCRETE NOT ACCEPTABLE.
 - JOINT SPACING TO BE 24'X24' (EVERY OTHER JOINT)

9 EXPANSION JOINT
NOT TO SCALE

2 PAVEMENT MARKINGS - 2
NOT TO SCALE

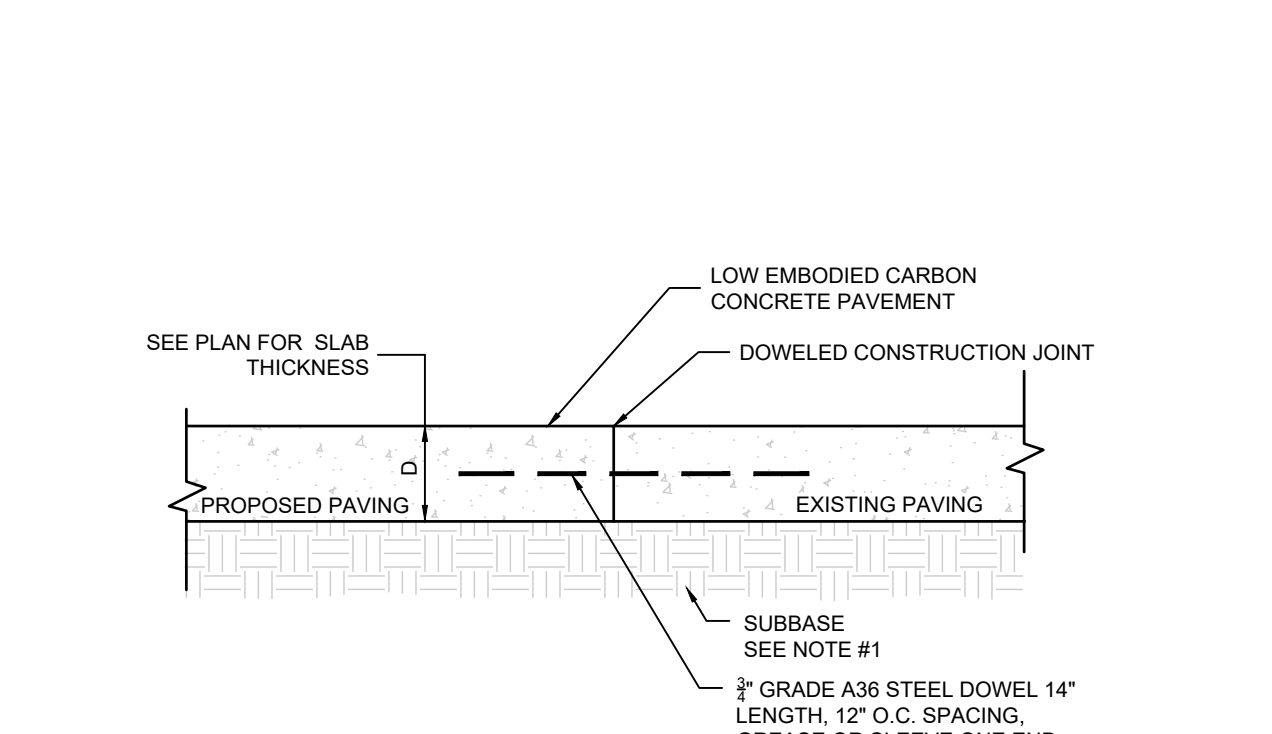
- NOTES:**
- GENERAL CONTRACTOR SHALL REFER TO PARKING LOT STRIPING SPECIFICATIONS. SEE DETAIL.
 - PAVEMENT MARKINGS SHALL BE APPLIED ACCORDING TO REQUIREMENTS AS OUTLINED IN SECTION 3B OF THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - CONTRACTOR SHALL USE WHITE REFLECTIVE PAINT ON ASPHALT & YELLOW REFLECTIVE PAINT ON CONCRETE, UNLESS UPON VERIFICATION BY THE GENERAL CONTRACTOR IT IS DETERMINED THAT LOCAL, STATE, OR ADA CODES DIFFER, IN WHICH CASE THESE CODES SHALL GOVERN.
 - IF STOP SIGNS ARE PROPOSED, "STOP" LETTERING ON STOP BAR DETAIL IS NOT REQUIRED.

2 PAVEMENT MARKINGS - 2
NOT TO SCALE



A TYPE "B" CURB & GUTTER **B TYPE "H" MOUNTABLE CURB & GUTTER**

6 CONCRETE CURB & GUTTER
NOT TO SCALE



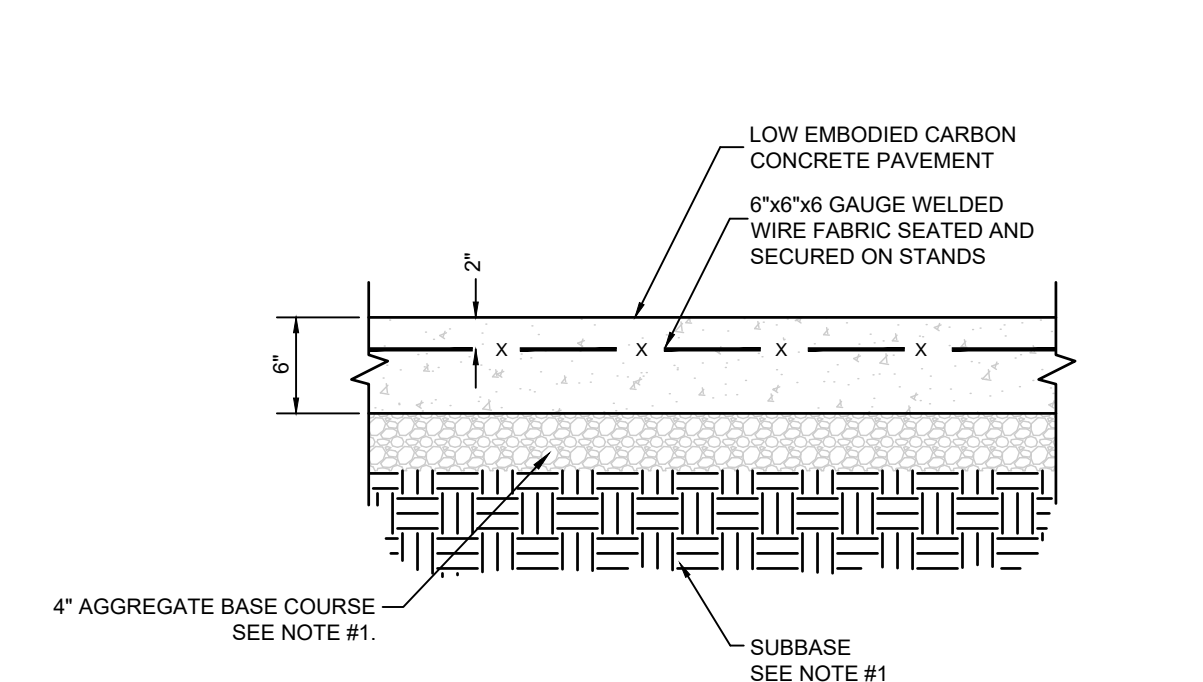
- NOTE:**
- GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS.

10 TRANSVERSE AND LONGITUDINAL DOWELED CONSTRUCTION JOINT
NOT TO SCALE

3 64° PARKING STRIPING
NOT TO SCALE

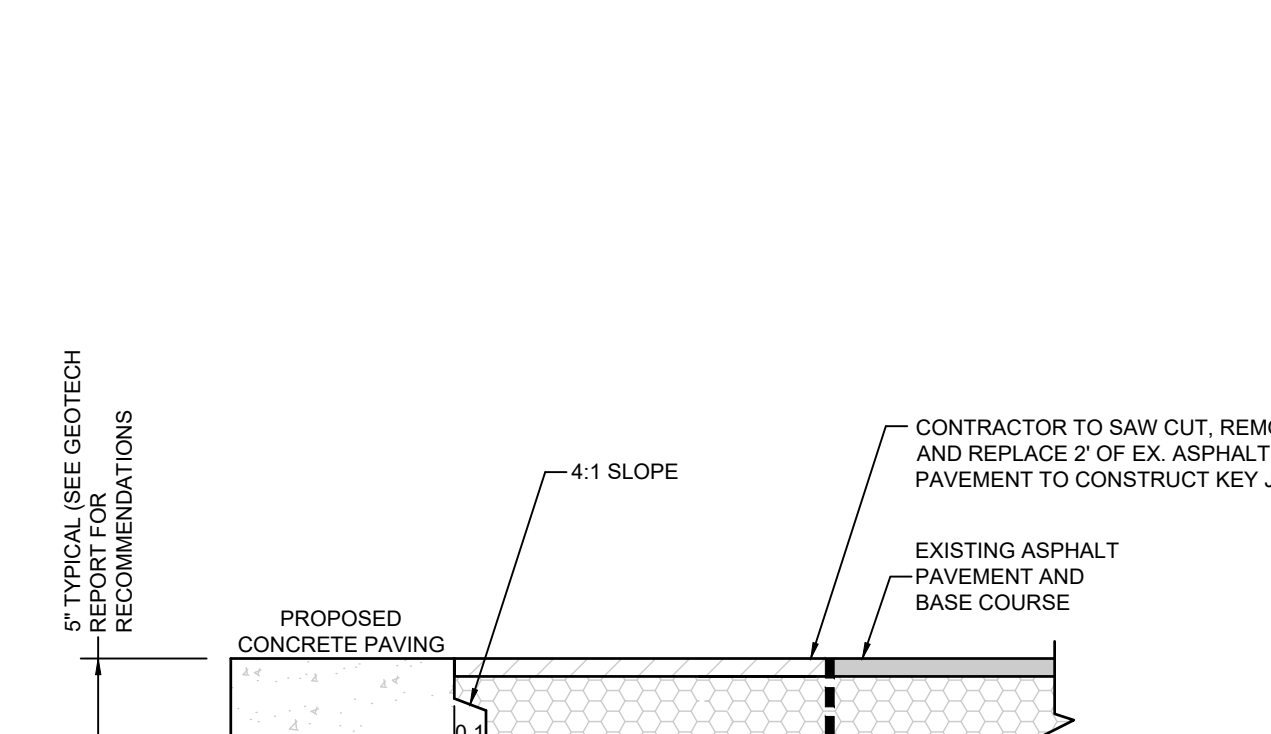
- NOTES:**
- ACCESSIBLE PARKING AND ACCESSIBLE AISLES SHALL NOT EXCEED 2% IN SLOPE IN ANY DIRECTION. IF ONLY ONE ACCESS ISLE IS INSTALLED, IT IS TO BE A VAN SIZE.
 - PARKING STALL DIMENSIONING SHALL BE IN ACCORDANCE WITH APPLICABLE GOVERNING AUTHORITIES & ADA STANDARDS. SEE SITE PLAN FOR COMPLETE STRIPING LAYOUT.
 - GENERAL CONTRACTOR SHALL REFER TO PARKING LOT STRIPING SPECIFICATIONS.
 - CONTRACTOR SHALL USE 4" WIDE WHITE REFLECTIVE PAINT FOR STRIPING ON ASPHALT PARKING LOTS.
 - CONTRACTOR SHALL USE 4" WIDE YELLOW REFLECTIVE PAINT FOR STRIPING ON CONCRETE PARKING LOTS.
 - NO WHEEL STOPS TO BE INSTALLED WHEN PARKING IS ADJACENT TO SIDEWALK.
 - ADA SIGNS IN BOLLARDS AND BOLLARDS SHALL BE INSTALLED WHEN PARKING IS ADJACENT TO FLUSH CURB OR A RAMP.
 - ALL DIMENSIONS ARE TO CENTERLINE OF STRIPE UNLESS NOTED OTHERWISE.
 - STRIPING IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR

3 64° PARKING STRIPING
NOT TO SCALE



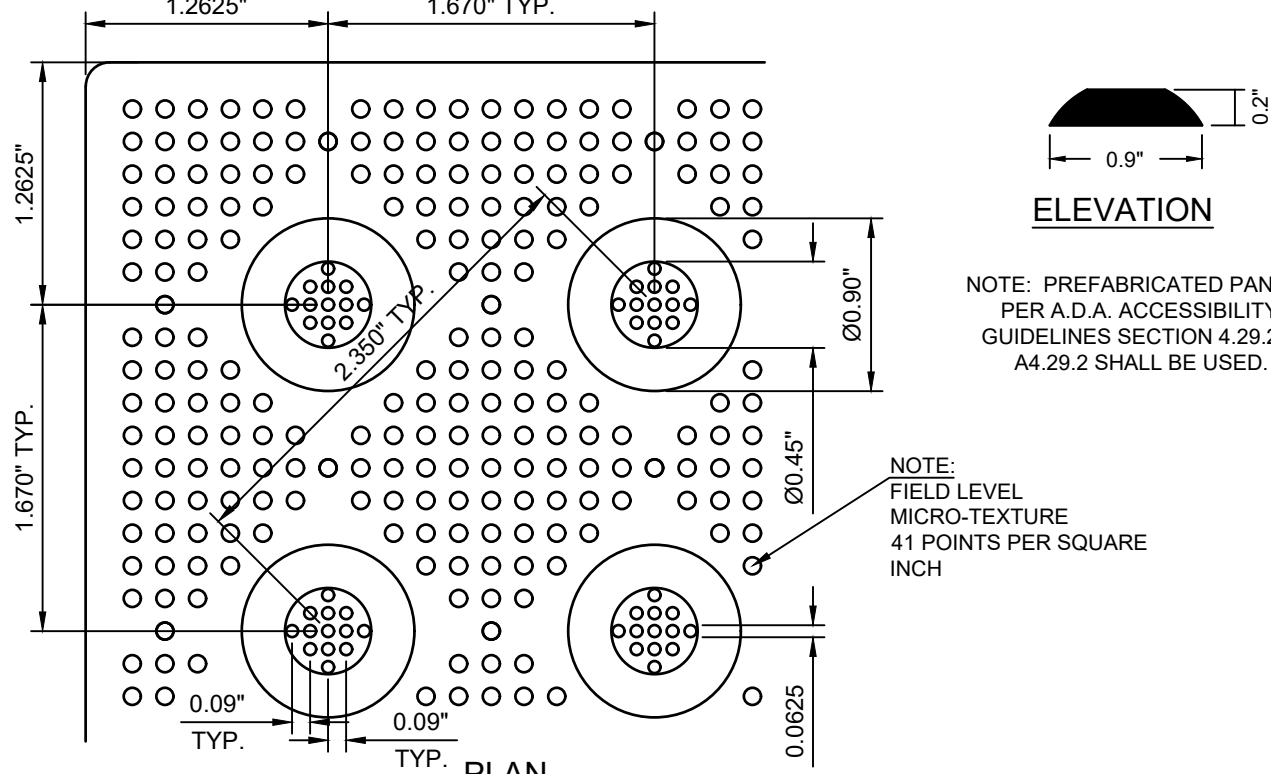
- NOTE:**
- GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS.
 - CONTRACTOR TO PROVIDE A SUSTAINABLE CONCRETE MIX FOR PAVEMENTS THAT MAY INCLUDE PORTLAND LESTONE CEMENT, OR ADDITIONAL CEMENTITIOUS MATERIALS INCLUDING FLY ASH OR SLAG CEMENT.

7 CONCRETE PAVEMENT DRIVE-THRU LANE
NOT TO SCALE



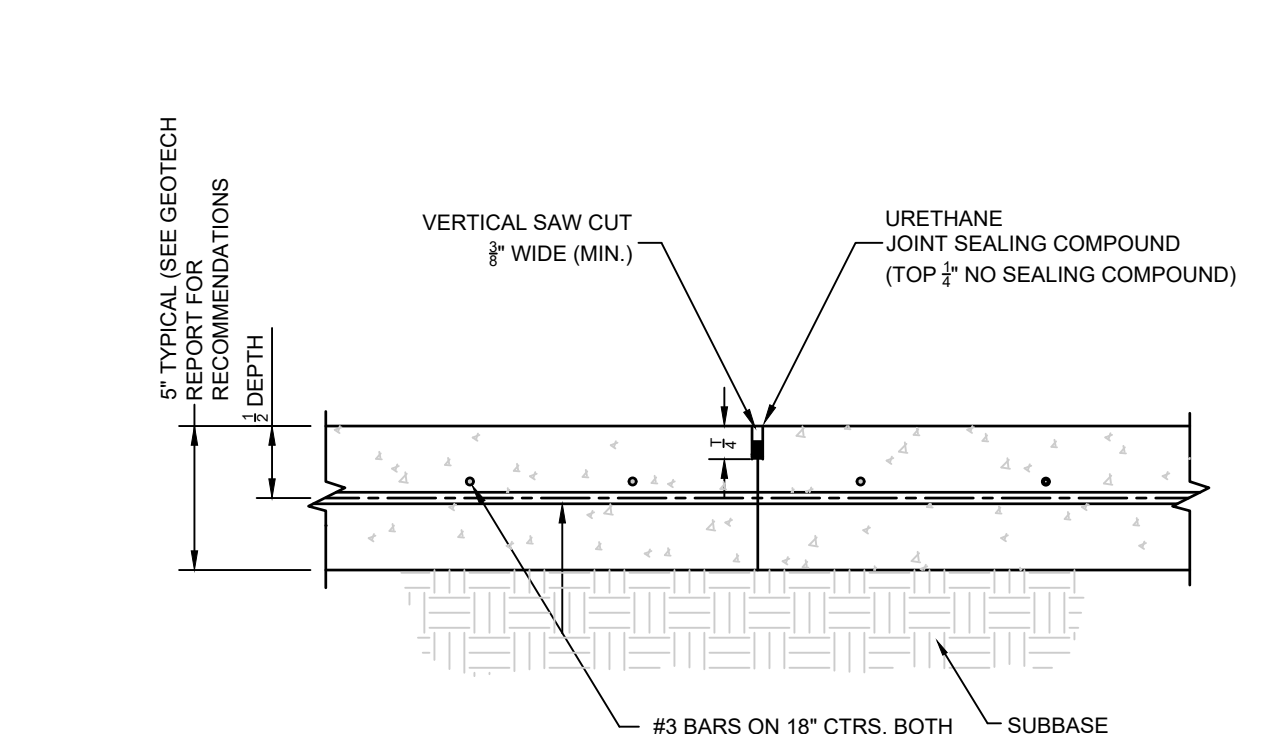
11 KEYED CONSTRUCTION JOINT
NOT TO SCALE

4 DIRECTIONAL SIGNAGE
NOT TO SCALE



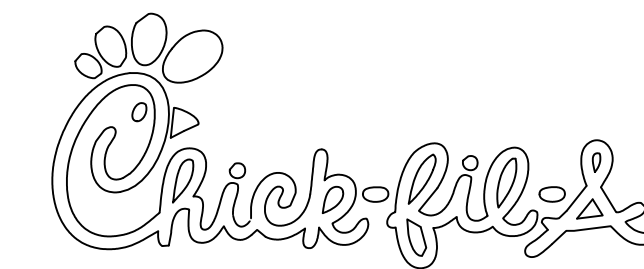
- NOTES:**
- THE DETECTABLE WARNING DEVICE SHALL BE LOCATED SO THAT THE NEAREST EDGE OF THE DEVICE IS 6 TO 8 INCHES FROM THE FACE OF CURB LINE.
 - DETECTABLE WARNING DEVICE SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND EXTEND THE FULL WIDTH OF THE RAMP OR FLUSH SURFACE.
 - DETECTABLE WARNING SURFACES SHALL BE CONSTRUCTED BY TEXTURING PRODUCTS CONFORMING TO PROWAG R304. TRANSITION SLOPES ARE NOT TO HAVE DETECTABLE WARNINGS. CONTRACTOR SHALL CONFIRM LOCAL CODES ARE MET.
 - WHERE A CURB RAMP IS CONSTRUCTED WITHIN AN EXISTING CURB, CURB & GUTTER AND/OR SIDEWALK, THE EXISTING CURB & GUTTER SHALL BE REMOVED TO THE NEAREST JOINT BEYOND THE CURB TRANSITIONS OR TO THE EXTENT THAT NO REMAINING SECTION OF CURB OR CURB & GUTTER IS LESS THAN 5' LONG. THE EXISTING SIDEWALK SHALL BE REMOVED TO THE NEAREST JOINT BEYOND THE TRANSITION SLOPE WALK AROUND OR TO THE EXTENT THAT NO REMAINING SECTION OF SIDEWALK IS LESS THAN 5'.
 - THE PLAN MUST PROVIDE FOR DETECTABLE WARNING SURFACE COLORS OR MATERIALS THAT PROVIDE THE NECESSARY CONTRAST, EITHER DARK-ON-LIGHT, OR LIGHT-ON-DARK. STANDARD DOME COLOR IS BRICK RED.
 - TRUNCATED DOMES TO BE INSTALLED USING ARMOR TILE CAST IN PLACE DOME TAG TILE. PART NUMBER ADA-2424 OR OTHER EQUIVALENT APPROVED MATERIAL. PREFERRED MANUFACTURER ARMOR TILE TACTILE SYSTEMS LANCE MITCHELL (919)622-4615 UNLESS PAVERS ARE REQUIRED, CONTRACTOR TO VERIFY THAT CURB RAMP MEET LOCAL CODES AND ADA REQUIREMENTS.

8 DETECTABLE WARNING DEVICE
NOT TO SCALE



- NOTES:**
- JOINT SPACING TO BE 12' x 12' WITH EVERY OTHER JOINT BEING AN EXPANSION JOINT.
 - GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS.

12 CONTRACTION JOINT
NOT TO SCALE



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MACEDONIA FSU
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FSR#2216

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SHEET NUMBER

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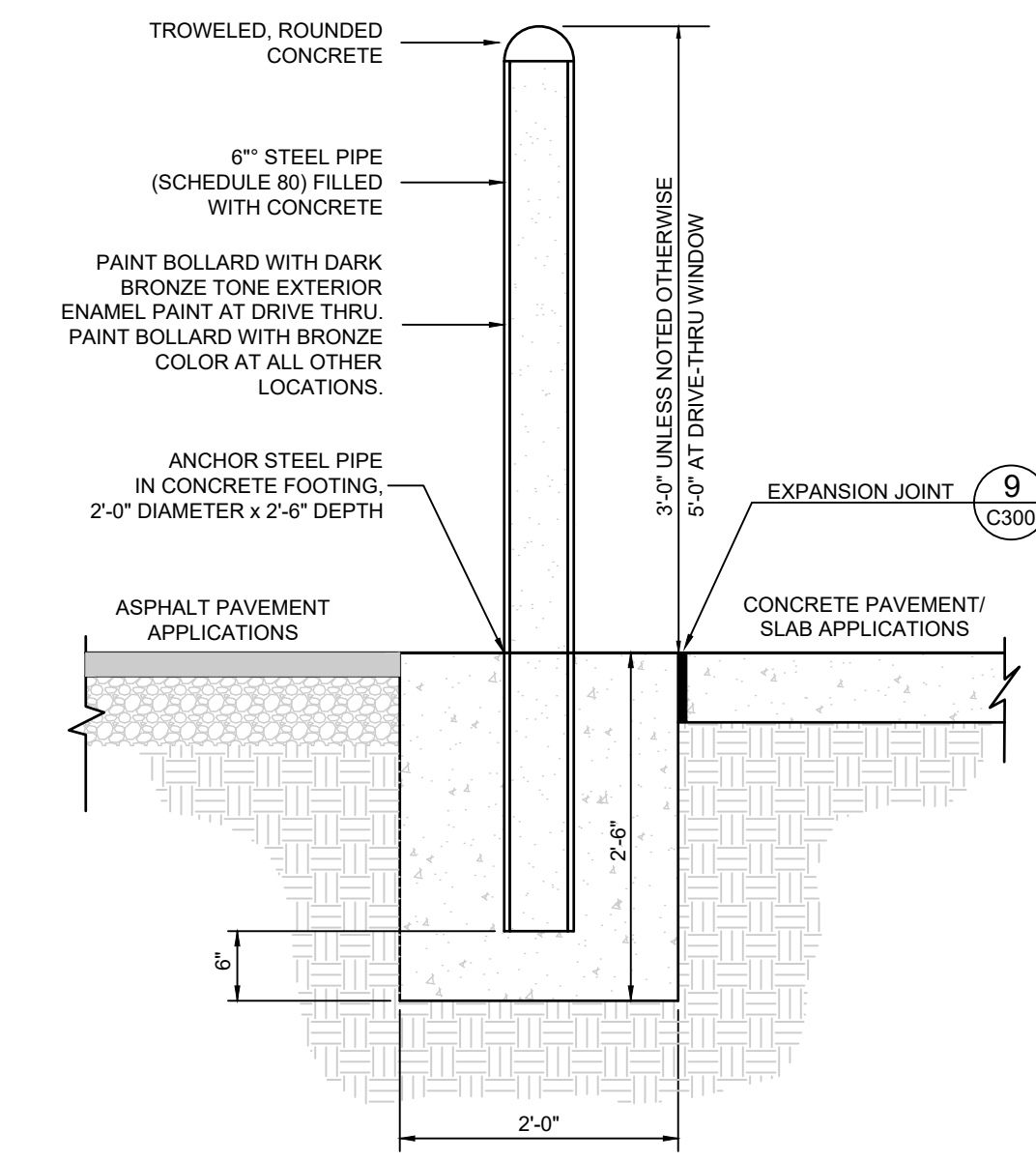
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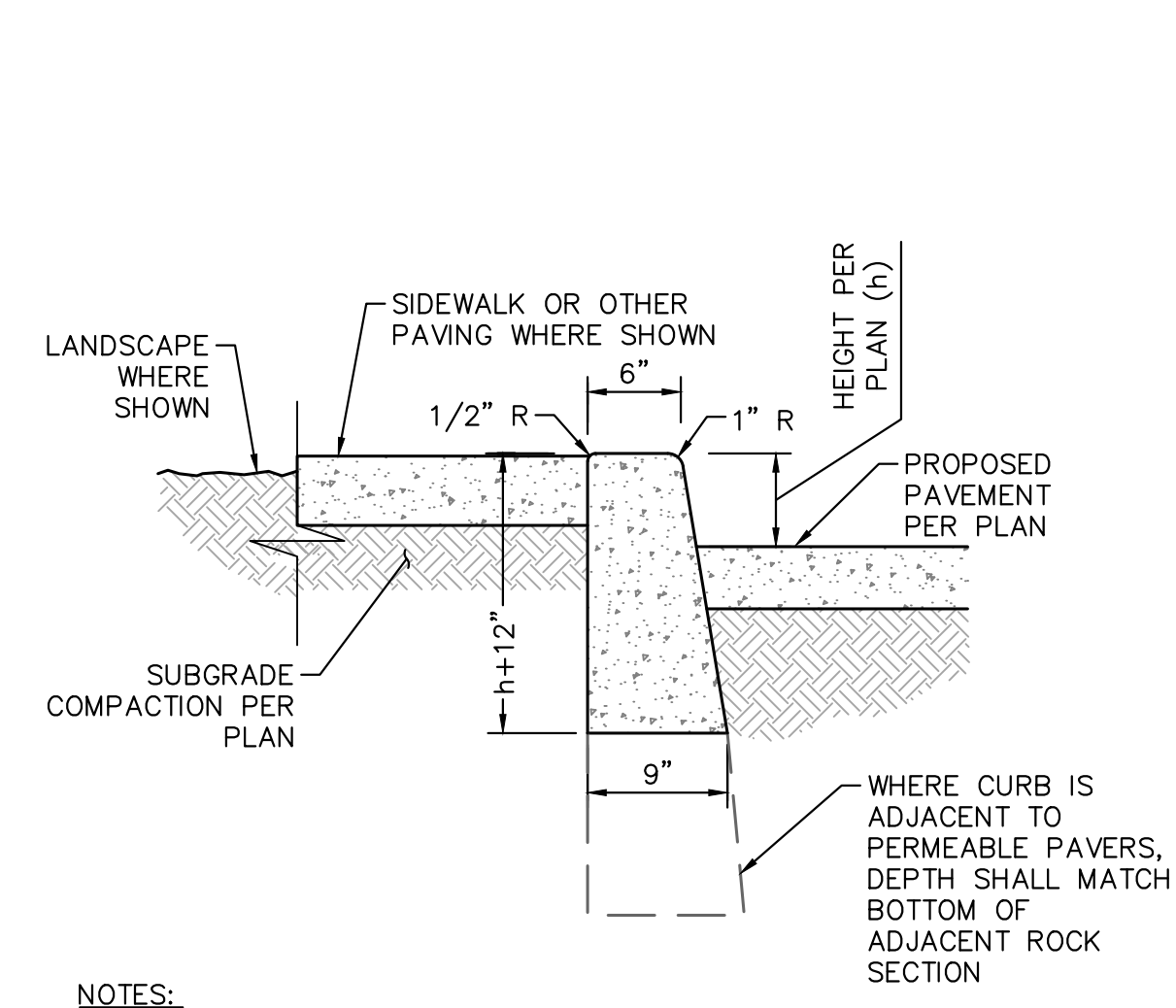
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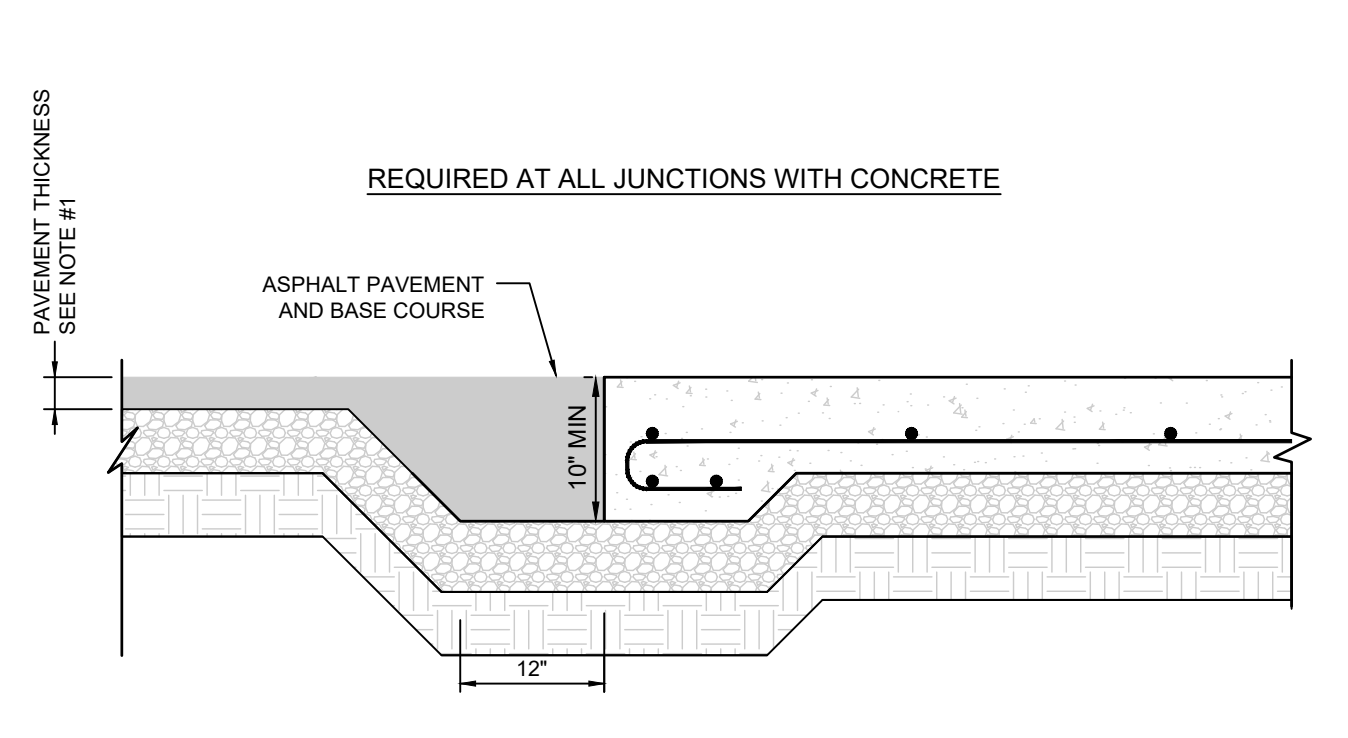
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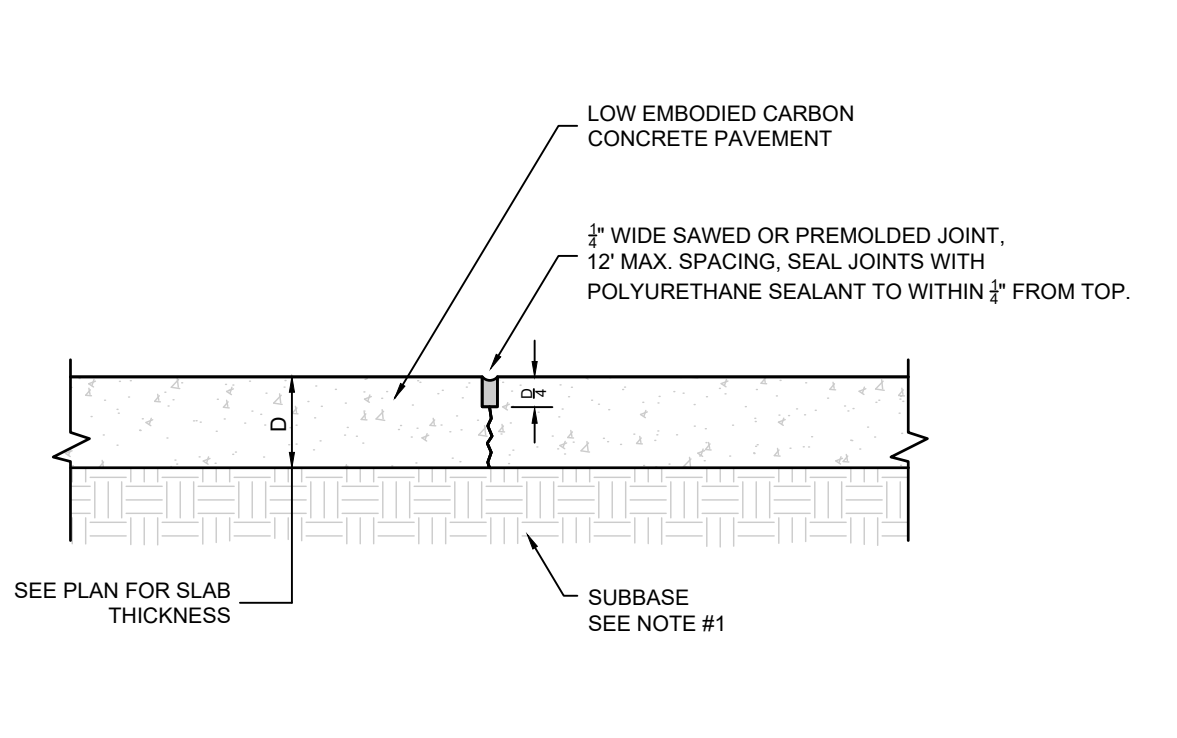
4 CONCRETE BOLLARD
NOT TO SCALE



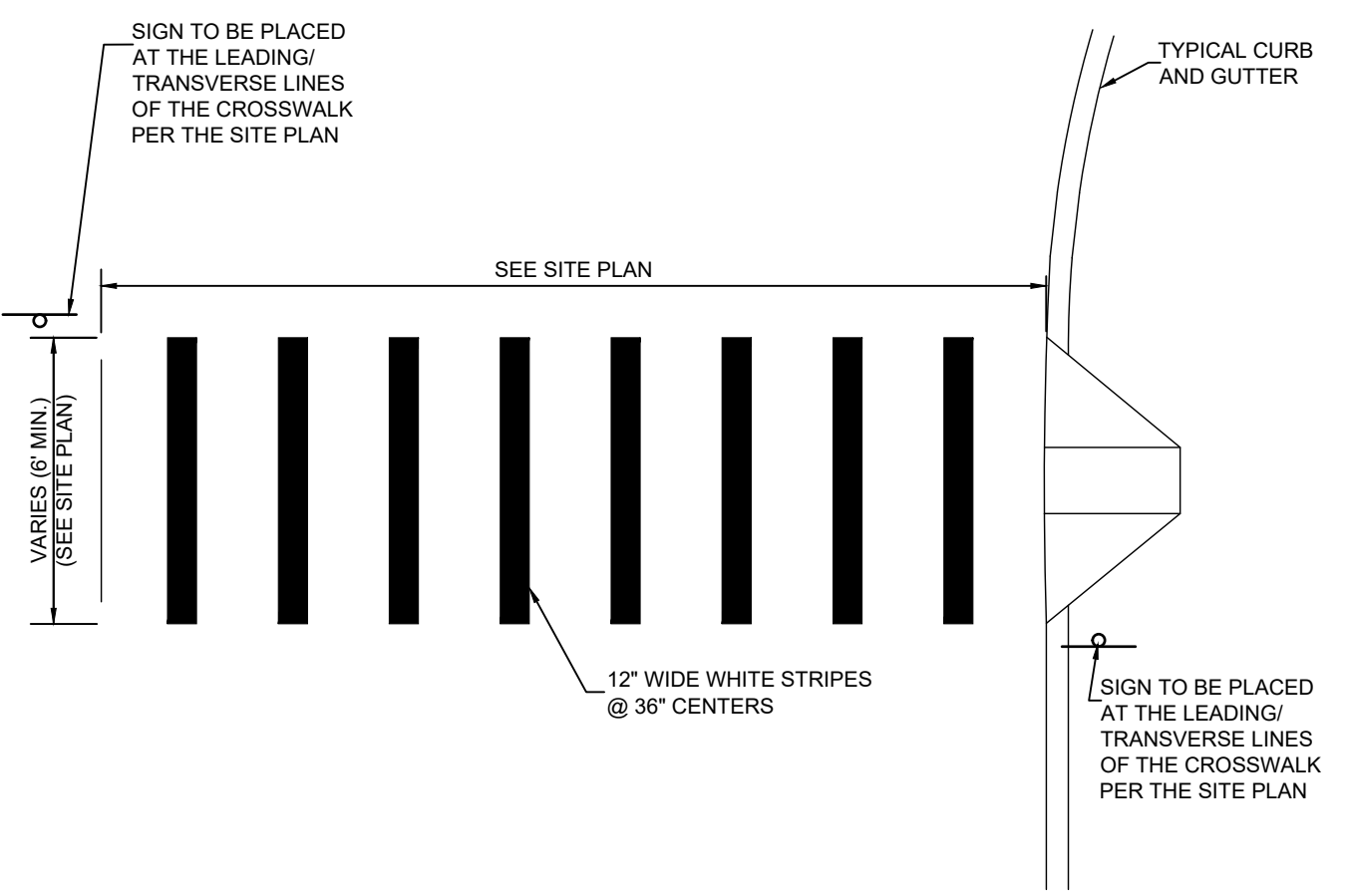
3 CONCRETE CURB
NOT TO SCALE



2 PAVEMENT EDGE DETAIL
NOT TO SCALE



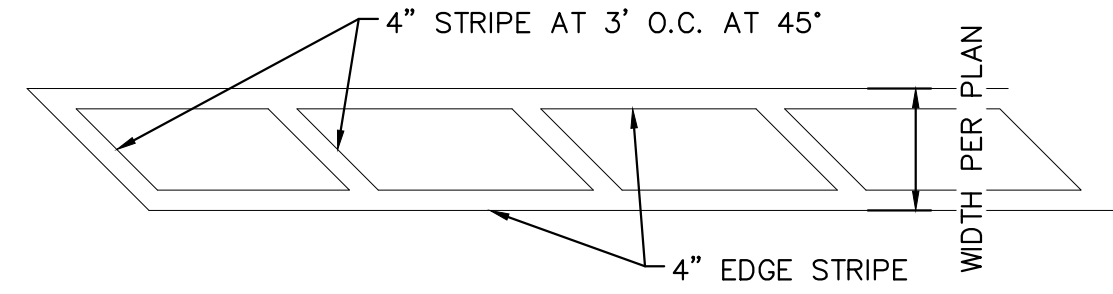
1 TRANSVERSE AND LONGITUDINAL CONTRACTION JOINT
NOT TO SCALE



5 CROSSWALK MARKINGS
NOT TO SCALE

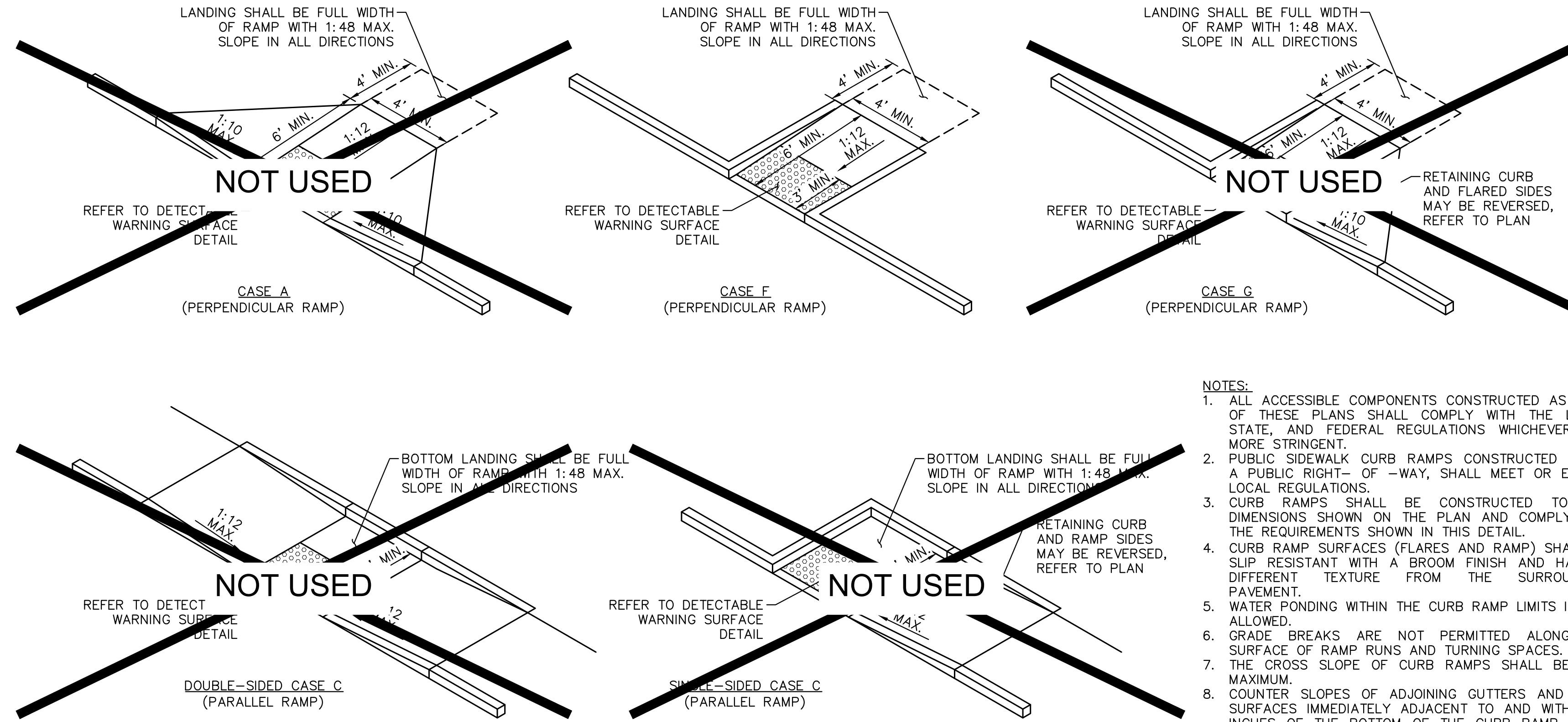
- NOTES:
- 1. MARKINGS FOR STREET SHALL BE ACCORDING TO LOCAL CODE REQUIREMENTS AND AS OUTLINED IN SECTION 3B OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.
 - 2. MARKINGS WITHIN PRIVATE PARKING LOT SHALL BE PER THIS DETAIL.
 - 3. THESE MARKINGS ARE TO BE PAINTED REFLECTIVE WHITE.

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15 May 2023

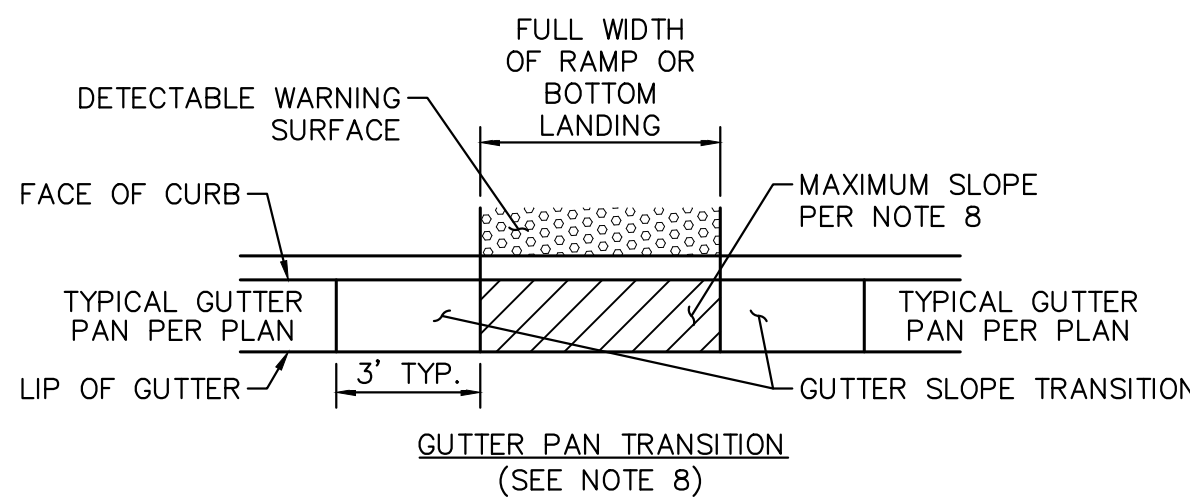


NOTES:
1. STRIPES SHALL BE YELLOW REFLECTIVE PAINT WITH ANTI-SLIP ADHESIVE.

1 DRIVE-THRU STRIPING TYPE 1
NOT TO SCALE

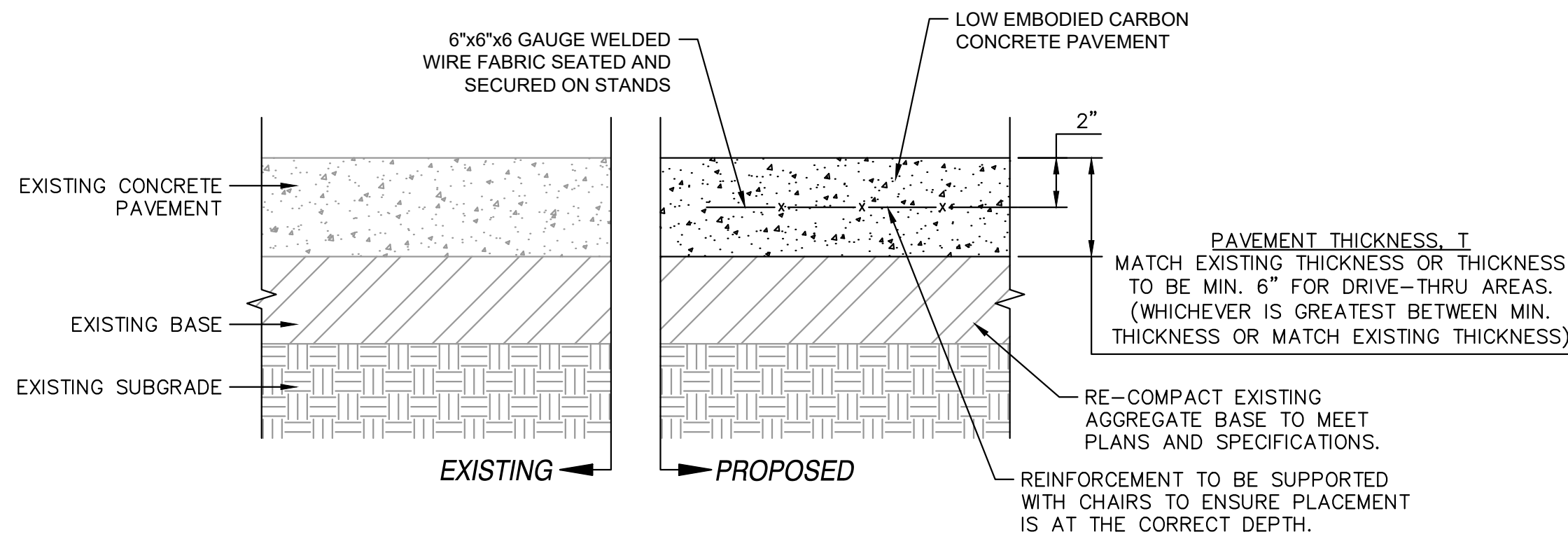


- NOTES:**
- ALL ACCESSIBLE COMPONENTS CONSTRUCTED AS PART OF THESE PLANS SHALL COMPLY WITH THE LOCAL, STATE, AND FEDERAL REGULATIONS WHICHEVER ARE MORE STRINGENT.
 - PUBLIC SIDEWALK CURB RAMPS CONSTRUCTED WITHIN A PUBLIC RIGHT-OF-WAY, SHALL MEET OR EXCEED LOCAL REGULATIONS.
 - CURB RAMPS SHALL BE CONSTRUCTED TO THE DIMENSIONS SHOWN ON THE PLAN AND COMPLY WITH THE REQUIREMENTS SHOWN IN THIS DETAIL.
 - CURB RAMP SURFACES (FLARES AND RAMP) SHALL BE SLIP RESISTANT WITH A BROOM FINISH AND HAVE A DIFFERENT TEXTURE FROM THE SURROUNDING PAVEMENT.
 - WATER PONDING WITHIN THE CURB RAMP LIMITS IS NOT ALLOWED.
 - GRADE BREAKS ARE NOT PERMITTED ALONG THE SURFACE OF RAMP RUNS AND TURNING SPACES.
 - THE CROSS SLOPE OF CURB RAMPS SHALL BE 1:48 MAXIMUM.
 - COUNTER SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO AND WITHIN 24 INCHES OF THE BOTTOM OF THE CURB RAMP SHALL NOT BE STEEPER THAN 1:20, EXCEPT WHERE ADJOINING THE ACCESS AISLE OF AN ACCESSIBLE PARKING STALL THE SLOPES SHALL NOT BE STEEPER THAN 1:48. WHERE NECESSARY GUTTER PAN SLOPES SHALL BE SOFTENED TO MEET THIS REQUIREMENT AND TRANSITION OVER A LENGTH OF 3' ON EITHER SIDE OF THE RAMP OR BOTTOM LANDING. GUTTER PAN TRANSITIONS SHALL MAINTAIN A CONSISTENT FLOWLINE GRADE.
 - THE ADJACENT SURFACES AT TRANSITIONS AT CURB RAMPS TO WALKS, GUTTERS, AND STREETS SHALL BE AT THE SAME LEVEL.
 - INSTALL 1/4" EXPANSION JOINT FILLER MATERIAL BETWEEN A NEW CURB RAMP AND THE EXISTING SIDEWALKS.



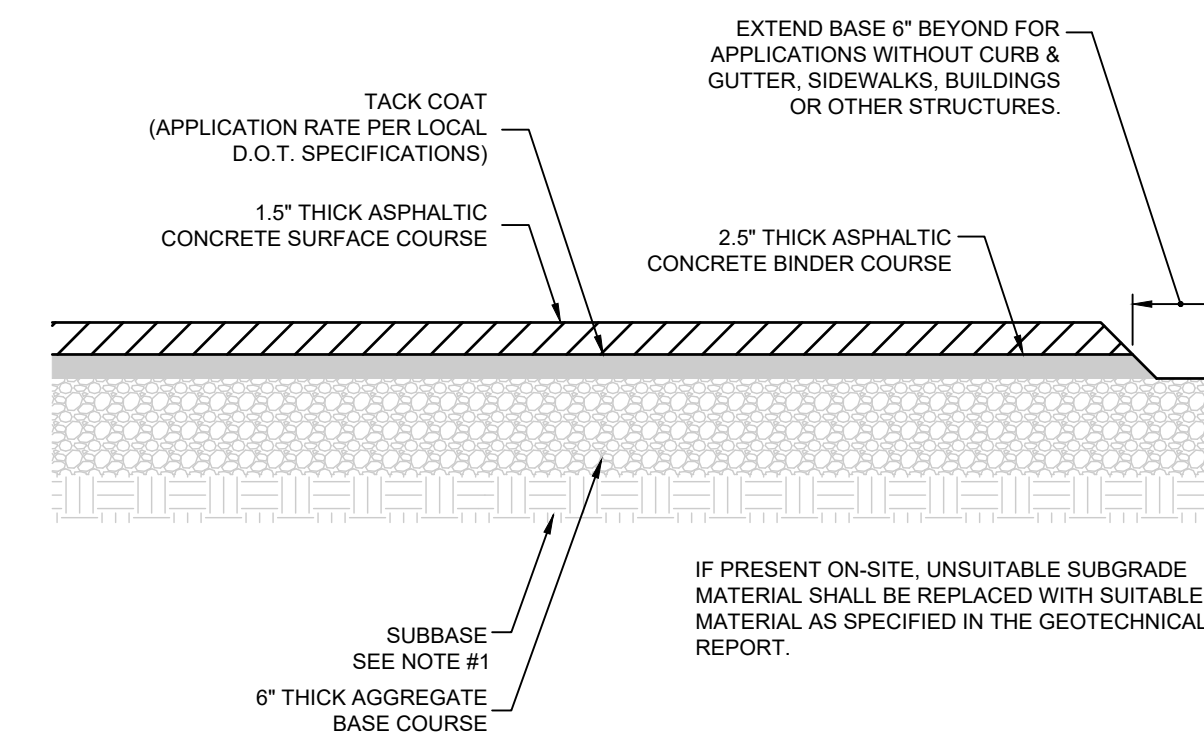
STANDARD ACCESSIBLE CURB RAMP
NOT TO SCALE

3



- NOTES:**
- IT SHALL BE REQUIRED TO USE A HIGH EARLY STRENGTH CONCRETE MIXTURE FOR SECTIONS OF PAVEMENT TO BE REPLACED AT HIGH PRIORITY DRIVE LANES THAT CANNOT BE PHASED OR DETOURED WITHOUT DISRUPTION OF OPERATIONS, AND ONLY THOSE LOADING ZONE AREAS THAT CANNOT BE SHIFTED TO ANOTHER DOOR LOCATION. IN ALL CASES, THE CONTRACTOR SHALL COORDINATE ALL CONCRETE REPAIR WORK WITH THE OWNER OR ENGINEER.
 - CONTRACTOR TO PROVIDE A SUSTAINABLE CONCRETE MIX FOR PAVEMENTS THAT MAY INCLUDE PORTLAND LESTONE CEMENT, OR ADDITIONAL CEMENTITIOUS MATERIALS INCLUDING FLY ASH OR SLAG CEMENT.
 - IF AGGREGATE BASE COURSE IS NOT PRESENT CONTRACTOR TO NOTIFY ENGINEER OF RECORD IMMEDIATELY.
 - EXISTING AGGREGATE BASE SHALL BE RE-COMPACTED TO 95% AS DETERMINED BY THE MODIFIED PROCTOR TEST (ASTM D-1557). MATERIALS TESTING AGENCY TO INSPECT PREPARED AGGREGATE BASE PRIOR TO PLACEMENT ON CONCRETE TO ENSURE COMPLIANCE WITH PROJECT PLANS AND SPECIFICATIONS.
 - REFER TO TYPICAL JOINTS FOR CONCRETE SLAB REPAIR DETAIL ON THIS SHEET FOR LOCATION AND TYPE OF JOINTS TO BE INSTALLED AROUND THE PERIMETER OF THE CONCRETE REPLACEMENT, DOWELS/REINFORCEMENT TO BE INSTALLED PER DETAILS ON THIS SHEET.
 - CONTRACTOR TO COORDINATE WITH OWNER ON TEST STRIP AS NEEDED TO MATCH EXISTING COLORED CONCRETE AS CLOSE AS POSSIBLE TO EXISTING.

2 CONCRETE PAVEMENT - REMOVE AND REPLACE CONCRETE PAVEMENT
NOT TO SCALE



- NOTE:**
- GENERAL CONTRACTOR SHALL REFERENCE SITE SPECIFIC GEOTECHNICAL REPORT FOR PAVEMENT, AGGREGATE, AND SUBGRADE SECTION REQUIREMENTS.
 - MINIMUM PAVEMENT THICKNESS SHOULD BE 6" GRADE AGGREGATE BASE, 2.5" ASPHALT PAVEMENT BINDER & 1.5" ASPHALT PAVEMENT SURFACE COURSE TACK COAT.

4 TYPICAL HMAC PAVEMENT SECTION
NOT TO SCALE



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GENERAL CONSTRUCTION NOTES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING ALL MATERIAL AND LABOR TO CONSTRUCT THE FACILITY AS SHOWN AND DESCRIBED IN THE CONSTRUCTION DOCUMENTS IN ACCORDANCE WITH THE APPROPRIATE APPROVING AUTHORITIES, SPECIFICATIONS AND REQUIREMENTS. CONTRACTOR SHALL CLEAR AND GRUB ALL AREAS UNLESS OTHERWISE INDICATED, REMOVING TREES, STUMPS, ROOTS, MUCK, EXISTING PAVEMENT AND ALL OTHER DELETERIOUS MATERIAL.
- EXISTING UTILITIES SHOWN ARE LOCATED ACCORDING TO THE INFORMATION AVAILABLE TO THE ENGINEER AT THE TIME OF THE TOPOGRAPHIC SURVEY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR THE ENGINEER. GUARANTEE IS NOT MADE THAT ALL EXISTING UNDERGROUND UTILITIES ARE SHOWN OR THAT THE LOCATION OF THOSE SHOWN ARE ENTIRELY ACCURATE. FINDING THE ACTUAL LOCATION OF ANY EXISTING UTILITIES IS THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE DONE BEFORE HE COMMENCES ANY WORK IN THE VICINITY. FURTHERMORE, THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES DUE TO THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. THE OWNER OR ENGINEER WILL ASSUME NO LIABILITY FOR ANY DAMAGES SUSTAINED OR COST INCURRED BECAUSE OF THE OPERATIONS IN THE VICINITY OF EXISTING UTILITIES OR STRUCTURES, NOR FOR TEMPORARY BRACING AND SHORING OF SAME. IF IT IS NECESSARY TO SHORE, BRACE, SWING OR RELOCATE A UTILITY, THE UTILITY COMPANY OR DEPARTMENT AFFECTED SHALL BE CONTACTED AND THEIR PERMISSION OBTAINED REGARDING THE METHOD TO USE FOR SUCH WORK.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES WHICH MAY HAVE BURIED OR AERIAL UTILITIES WITHIN OR NEAR THE CONSTRUCTION AREA BEFORE COMMENCING WORK. THE CONTRACTOR SHALL PROVIDE 48 HOURS MINIMUM NOTICE TO ALL UTILITY COMPANIES PRIOR TO BEGINNING CONSTRUCTION. A LIST OF THE UTILITY COMPANIES WHICH THE CONTRACTOR MUST CALL BEFORE COMMENCING WORK IS PROVIDED ON THE COVER SHEET OF THESE CONSTRUCTION PLANS. THIS LIST SERVES AS A GUIDE ONLY AND IS NOT INTENDED TO LIMIT THE UTILITY COMPANIES WHICH THE CONTRACTOR MAY WISH TO NOTIFY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED CONSTRUCTION PERMITS AND BONDS IF REQUIRED PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL HAVE AVAILABLE AT THE JOB SITE AT ALL TIMES ONE COPY OF THE CONSTRUCTION DOCUMENTS INCLUDING PLANS, SPECIFICATIONS, AND SPECIAL CONDITIONS AND COPIES OF ANY REQUIRED CONSTRUCTION PERMITS.
- ANY DISCREPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER BEFORE COMMENCING WORK. NO FIELD CHANGES OR DEVIATIONS FROM DESIGN ARE TO BE MADE WITHOUT PRIOR APPROVAL OF THE OWNER AND NOTIFICATION TO THE ENGINEER.
- ALL COPIES OF COMPACTION, CONCRETE AND OTHER REQUIRED TEST RESULTS ARE TO BE SENT TO THE OWNER AND DESIGN ENGINEER OF RECORD DIRECTLY FROM THE TESTING AGENCY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING TO THE ENGINEER A CERTIFIED RECORD SURVEY SIGNED AND SEALED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF OHIO DEPICTING THE ACTUAL FIELD LOCATION OF ALL CONSTRUCTED IMPROVEMENTS THAT ARE REQUIRED BY THE JURISDICTIONAL AGENCIES FOR THE CERTIFICATION PROCESS. ALL SURVEY COSTS WILL BE THE CONTRACTOR'S RESPONSIBILITY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DOCUMENTING AND MAINTAINING AS-BUILT INFORMATION WHICH SHALL BE RECORDED AS CONSTRUCTION PROGRESSES OR AT THE COMPLETION OF APPROPRIATE CONSTRUCTION INTERVALS AND SHALL BE RESPONSIBLE FOR PROVIDING AS-BUILT DRAWINGS TO THE OWNER FOR THE PURPOSE OF CERTIFICATION TO JURISDICTIONAL AGENCIES AS REQUIRED. ALL AS-BUILT DATA SHALL BE COLLECTED BY A STATE OF OHIO PROFESSIONAL LAND SURVEYOR WHOSE SERVICES ARE ENGAGED BY THE CONTRACTOR.
- ANY WELLS DISCOVERED ON SITE THAT WILL HAVE NO USE MUST BE PLUGGED BY A LICENSED WELL DRILLING CONTRACTOR IN A MANNER APPROVED BY ALL JURISDICTIONAL AGENCIES. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY WELL ABANDONMENT PERMITS REQUIRED.
- ANY WELL DISCOVERED DURING EARTH MOVING OR EXCAVATION SHALL BE REPORTED TO THE APPROPRIATE JURISDICTIONAL AGENCIES WITHIN 24 HOURS AFTER DISCOVERY IS MADE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THAT THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS DO NOT CONFLICT WITH ANY KNOWN EXISTING OR OTHER PROPOSED IMPROVEMENTS. IF ANY CONFLICTS ARE DISCOVERED, THE CONTRACTOR SHALL NOTIFY THE OWNER PRIOR TO INSTALLATION OF ANY PORTION OF THE SITE WORK THAT WOULD BE AFFECTED. FAILURE TO NOTIFY OWNER OF AN IDENTIFIABLE CONFLICT PRIOR TO PROCEEDING WITH INSTALLATION RELIEVES OWNER OF ANY OBLIGATION TO PAY FOR A RELATED CHANGE ORDER.

DEMOLITION NOTES

- ALL MATERIAL REMOVED FROM THIS SITE BY THE CONTRACTOR SHALL BE DISPOSED OF BY THE CONTRACTOR IN A LEGAL MANNER.
- REFER TO THE TOPOGRAPHIC SURVEY FOR ADDITIONAL DETAILS OF EXISTING STRUCTURES, ETC., LOCATED WITHIN THE PROJECT SITE. UNLESS OTHERWISE NOTED, ALL EXISTING BUILDINGS, STRUCTURES, SLABS, CONCRETE, ASPHALT, DEBRIS PILES, SIGNS, AND ALL APPURTENANCES ARE TO BE REMOVED FROM THE SITE BY THE CONTRACTOR AND PROPERLY DISPOSED OF IN A LEGAL MANNER AS PART OF THIS CONTRACT. SOME ITEMS TO BE REMOVED MAY NOT BE DEPICTED ON THE TOPOGRAPHIC SURVEY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND DETERMINE THE FULL EXTENT OF ITEMS TO BE REMOVED. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE OWNER PRIOR TO REMOVAL OF SAID ITEMS.
- THE CONTRACTOR SHALL REFER TO THE DEMOLITION PLAN FOR DEMOLITION/PRESERVATION OF EXISTING TREES. ALL TREES NOT SPECIFICALLY SHOWN TO BE PRESERVED OR RELOCATED SHALL BE REMOVED AS A PART OF THIS CONTRACT. TREE PROTECTION FENCING SHALL BE INSTALLED PRIOR TO ANY DEMOLITION.

PAVING, GRADING AND DRAINAGE NOTES

- ALL PAVING, CONSTRUCTION, MATERIALS, AND WORKMANSHIP WITHIN COUNTY'S RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH LOCAL OR COUNTY SPECIFICATIONS AND STANDARDS (LATEST EDITION) OR ODOT SPECIFICATIONS AND STANDARDS (LATEST EDITION) IF NOT COVERED BY LOCAL OR COUNTY REGULATIONS.
 - ALL UNPAVED AREAS IN EXISTING RIGHTS-OF-WAY DISTURBED BY CONSTRUCTION SHALL BE REGRADED AND SODDED.
 - TRAFFIC CONTROL ON ALL ODOT, LOCAL AND COUNTY RIGHTS-OF-WAY SHALL MEET THE REQUIREMENTS OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (U.S. DOT/FHA) AND THE REQUIREMENTS OF THE STATE AND ANY LOCAL AGENCY HAVING JURISDICTION. IN THE EVENT THAT THE CONTRACT DOCUMENTS AND THE JURISDICTIONAL AGENCY REQUIREMENTS ARE NOT IN AGREEMENT, THE MOST STRINGENT SHALL GOVERN.
 - THE CONTRACTOR SHALL GRADE THE SITE TO THE ELEVATIONS INDICATED AND SHALL REGRADE WASHOUTS WHERE THEY OCCUR AFTER EVERY RAINFALL UNTIL A GRASS STAND IS WELL ESTABLISHED OR ADEQUATE STABILIZATION OCCURS.
 - ALL OPEN AREAS WITHIN THE PROJECT SITE SHALL BE SODDED UNLESS INDICATED OTHERWISE ON THE LANDSCAPE PLAN.
 - ALL AREAS INDICATED AS PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE TYPICAL PAVEMENT SECTIONS AS INDICATED ON THE DRAWINGS.
 - WHERE EXISTING PAVEMENT IS INDICATED TO BE REMOVED AND REPLACED, THE CONTRACTOR SHALL SAW CUT A MINIMUM 2" DEEP FOR A SMOOTH AND STRAIGHT JOINT AND REPLACE THE PAVEMENT WITH THE SAME TYPE AND DEPTH OF MATERIAL AS EXISTING OR AS INDICATED.
 - WHERE NEW PAVEMENT MEETS THE EXISTING PAVEMENT, THE CONTRACTOR SHALL SAW CUT THE EXISTING PAVEMENT A MINIMUM 2" DEEP FOR A SMOOTH AND STRAIGHT JOINT AND MATCH THE EXISTING PAVEMENT ELEVATION WITH THE PROPOSED PAVEMENT UNLESS OTHERWISE INDICATED.
 - THE CONTRACTOR SHALL INSTALL FILTER FABRIC OVER ALL DRAINAGE STRUCTURES FOR THE DURATION OF CONSTRUCTION AND UNTIL ACCEPTANCE OF THE PROJECT BY THE OWNER. ALL DRAINAGE STRUCTURES SHALL BE CLEANED OF DEBRIS AS REQUIRED DURING AND AT THE END OF CONSTRUCTION TO PROVIDE POSITIVE DRAINAGE FLOWS.
 - IF DEWATERING IS REQUIRED, THE CONTRACTOR SHALL OBTAIN ANY APPLICABLE REQUIRED PERMITS. THE CONTRACTOR IS TO COORDINATE WITH THE OWNER AND THE DESIGN ENGINEER PRIOR TO ANY EXCAVATION.
 - STRIP TOPSOIL AND ORGANIC MATTER FROM ALL AREAS OF THE SITE AS REQUIRED. IN SOME CASES TOPSOIL MAY BE STOCKPILED ON SITE FOR PLACEMENT WITHIN LANDSCAPED AREAS BUT ONLY AS DIRECTED BY THE OWNER.
 - FIELD DENSITY TESTS SHALL BE TAKEN AT INTERVALS IN ACCORDANCE WITH THE LOCAL JURISDICTIONAL AGENCY OR TO ODOT STANDARDS. IN THE EVENT THAT THE CONTRACT DOCUMENTS AND THE JURISDICTIONAL AGENCY REQUIREMENTS ARE NOT IN AGREEMENT, THE MOST STRINGENT SHALL GOVERN.
 - ALL SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED AS PER PLANS. THE AREAS SHALL THEN BE SODDED OR SEEDED AS SPECIFIED IN THE PLANS, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE JOB SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. ALL EARTHEN AREAS WILL BE SODDED OR SEEDED AND MULCHED AS SHOWN ON THE LANDSCAPING PLAN.
 - ALL CUT OR FILL SLOPES SHALL BE 4 (HORIZONTAL) : 1 (VERTICAL) OR FLATTER UNLESS OTHERWISE SHOWN.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTROL OF DUST AND DIRT RISING AND SCATTERING IN THE AIR DURING CONSTRUCTION AND SHALL PROVIDE WATER SPRINKLING OR OTHER SUITABLE METHODS OF CONTROL. THE CONTRACTOR SHALL COMPLY WITH ALL GOVERNING REGULATIONS PERTAINING TO ENVIRONMENTAL PROTECTION.
- THE CONTRACTOR SHALL TAKE ALL REQUIRED MEASURES TO CONTROL TURBIDITY, INCLUDING BUT NOT LIMITED TO THE INSTALLATION OF TURBIDITY BARRIERS AT ALL LOCATIONS WHERE THE POSSIBILITY OF TRANSFERRING SUSPENDED SOLIDS INTO THE RECEIVING WATER BODY EXISTS DUE TO THE PROPOSED WORK. TURBIDITY BARRIERS MUST BE MAINTAINED IN EFFECTIVE CONDITION AT ALL LOCATIONS UNTIL CONSTRUCTION IS COMPLETED AND DISTURBED SOIL AREAS ARE STABILIZED. THEREAFTER, THE CONTRACTOR MUST REMOVE THE BARRIERS. AT NO TIME SHALL THERE BE ANY OFF-SITE DISCHARGE WHICH VIOLATES THE WATER QUALITY STANDARDS PER OHIO DEPARTMENT OF HEALTH AND ENVIRONMENT.
- SOD, WHERE CALLED FOR, MUST BE INSTALLED AND MAINTAINED ON EXPOSED SLOPES WITHIN 48 HOURS OF COMPLETING FINAL GRADING, AND AT ANY OTHER TIME AS NECESSARY, TO PREVENT EROSION, SEDIMENTATION OR TURBID DISCHARGES.
 - THE CONTRACTOR MUST REVIEW AND MAINTAIN A COPY OF THE ENVIRONMENTAL RESOURCE PERMIT COMPLETE WITH ALL CONDITIONS, ATTACHMENTS, EXHIBITS, AND PERMIT MODIFICATIONS IN GOOD CONDITION AT THE CONSTRUCTION SITE. THE COMPLETE PERMIT MUST BE AVAILABLE FOR REVIEW UPON REQUEST BY WATER MANAGEMENT DISTRICT REPRESENTATIVES.
 - THE CONTRACTOR SHALL ENSURE THAT ISLAND PLANTING AREAS AND OTHER PLANTING AREAS ARE NOT COMPACTED AND DO NOT CONTAIN ROAD BASE MATERIALS. THE CONTRACTOR SHALL ALSO EXCAVATE AND REMOVE ALL UNDESIRABLE MATERIAL FROM ALL AREAS ON THE SITE TO BE PLANTED AND PROPERLY DISPOSED OF IN A LEGAL MANNER.
 - THE CONTRACTOR SHALL INSTALL ALL UNDERGROUND STORM WATER PIPING PER MANUFACTURER'S RECOMMENDATIONS.
 - ALL CONCRETE WITHIN THE PUBIC RIGHT-OF-WAY SHALL BE KOMMB4K.

WATER AND SEWER UTILITY NOTES

- THE CONTRACTOR SHALL CONSTRUCT GRAVITY SEWER LATERALS, MANHOLES GRAVITY SEWER LINES AND DOMESTIC WATER AND FIRE PROTECTION SYSTEM AS SHOWN ON THESE PLANS. THE CONTRACTOR SHALL FURNISH ALL NECESSARY MATERIALS, EQUIPMENT, MACHINERY, TOOLS, MEANS OF TRANSPORTATION AND LABOR NECESSARY TO COMPLETE THE WORK IN FULL AND COMPLETE ACCORDANCE WITH THE SHOWN, DESCRIBED AND REASONABLY INTENDED REQUIREMENTS OF THE CONTRACT DOCUMENTS AND JURISDICTIONAL AGENCY REQUIREMENTS. IN THE EVENT THAT THE CONTRACT DOCUMENTS AND THE JURISDICTIONAL AGENCY REQUIREMENTS ARE NOT IN AGREEMENT, THE MOST STRINGENT SHALL GOVERN.
 - ALL EXISTING UNDERGROUND UTILITY LOCATIONS SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS FOR UTILITY LOCATION AND COORDINATION IN ACCORDANCE WITH THE NOTES CONTAINED IN THE GENERAL CONSTRUCTION SECTION OF THIS SHEET.
 - THE CONTRACTOR SHALL RESTORE ALL DISTURBED VEGETATION IN KIND, UNLESS SHOWN OTHERWISE.
 - DEFLECTION OF PIPE JOINTS AND CURVATURE OF PIPE SHALL NOT EXCEED THE MANUFACTURER'S SPECIFICATIONS. SECURELY CLOSE ALL OPEN ENDS OF PIPE AND FITTINGS WITH A WATERTIGHT PLUG WHEN WORK IS NOT IN PROGRESS. THE INTERIOR OF ALL PIPES SHALL BE CLEAN AND JOINT SURFACES WIPE CLEAN AND DRY AFTER THE PIPE HAS BEEN LOWERED INTO THE TRENCH. VALVES SHALL BE PLUMB AND LOCATED ACCORDING TO THE PLANS.
 - ALL PHASES OF INSTALLATION, INCLUDING UNLOADING, TRENCHING, LAYING AND BACK FILLING, SHALL BE DONE IN A FIRST CLASS WORKMANLIKE MANNER. ALL PIPE AND FITTINGS SHALL BE CAREFULLY STORED FOLLOWING MANUFACTURER'S RECOMMENDATIONS. CARE SHALL BE TAKEN TO AVOID DAMAGE TO THE COATING OR LINING IN ANY D.I. PIPE FITTINGS. ANY PIPE OR FITTING WHICH IS DAMAGED OR WHICH HAS FLAWS OR IMPERFECTIONS WHICH, IN THE OPINION OF THE ENGINEER OR OWNER, RENDERS IT UNFIT FOR USE, SHALL NOT BE USED. ANY PIPE NOT SATISFACTORY FOR USE SHALL BE CLEARLY MARKED AND IMMEDIATELY REMOVED FROM THE JOB SITE, AND SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
 - WATER FOR FIRE FIGHTING SHALL BE AVAILABLE FOR USE PRIOR TO COMBUSTIBLES BEING BROUGHT ON SITE.
 - ALL UTILITY AND STORM DRAIN TRENCHES LOCATED UNDER AREAS TO RECEIVE PAVING SHALL BE COMPLETELY BACK FILLED IN ACCORDANCE WITH THE GOVERNING JURISDICTIONAL AGENCY'S SPECIFICATIONS. IN THE EVENT THAT THE CONTRACT DOCUMENTS AND THE JURISDICTIONAL AGENCY REQUIREMENTS ARE NOT IN AGREEMENT, THE MOST STRINGENT SHALL GOVERN.
 - UNDERGROUND LINES SHALL BE SURVEYED BY A STATE OF OHIO PROFESSIONAL LAND SURVEYOR PRIOR TO BACK FILLING.
 - CONTRACTOR SHALL PERFORM, AT HIS OWN EXPENSE, ANY AND ALL TESTS REQUIRED BY THE SPECIFICATIONS AND/OR ANY AGENCY HAVING JURISDICTION. THESE TESTS MAY INCLUDE, BUT MAY NOT BE LIMITED TO, INFILTRATION AND EXFILTRATION, TELEVISION INSPECTION AND A MANDREL TEST ON GRAVITY SEWER. A COPY OF THE TEST RESULTS SHALL BE PROVIDED TO THE UTILITY PROVIDER, OWNER AND JURISDICTIONAL AGENCY AS REQUIRED.
- EROSION CONTROL NOTES**
- THE STORM WATER POLLUTION PREVENTION PLAN ("SWPPP") IS COMPRISED OF THE EROSION CONTROL PLAN, THE STANDARD DETAILS, THE PLAN NARRATIVE, ATTACHMENTS INCLUDED IN SPECIFICATIONS OF THE SWPPP, PLUS THE PERMIT AND ALL SUBSEQUENT REPORTS AND RELATED DOCUMENTS.
 - ALL CONTRACTORS AND SUBCONTRACTORS INVOLVED WITH STORM WATER POLLUTION PREVENTION SHALL OBTAIN A COPY OF THE STORM WATER POLLUTION PREVENTION PLAN AND THE STATE OF OHIO CONSTRUCTION STORMWATER PERMIT AND BECOME FAMILIAR WITH THEIR CONTENTS.
 - THE CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES AS REQUIRED BY THE SWPPP. ADDITIONAL BEST MANAGEMENT PRACTICES SHALL BE IMPLEMENTED AS DICTATED BY CONDITIONS AT NO ADDITIONAL COST TO THE OWNER THROUGHOUT ALL PHASES OF CONSTRUCTION.
 - BEST MANAGEMENT PRACTICES (BMP'S) AND CONTROLS SHALL CONFORM TO FEDERAL, STATE, OR LOCAL REQUIREMENTS OR MANUAL OF PRACTICE, AS APPLICABLE. THE CONTRACTOR SHALL IMPLEMENT ADDITIONAL CONTROLS AS DIRECTED BY THE PERMITTING AGENCY OR OWNER.
 - EROSION CONTROL PLAN MUST CLEARLY DELINEATE ALL STATE WATERS. PERMITS FOR ANY CONSTRUCTION ACTIVITY IMPACTING STATE WATERS OR REGULATED WETLANDS MUST BE MAINTAINED ON SITE AT ALL TIMES.
 - THE CONTRACTOR SHALL MINIMIZE CLEARING TO THE MAXIMUM EXTENT PRACTICAL OR AS REQUIRED BY THE GENERAL PERMIT.
 - CONTRACTOR SHALL DENOTE ON PLAN THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA, EMPLOYEE PARKING AREA, AND AREA FOR LOCATING PORTABLE FACILITIES, OFFICE TRAILERS, AND TOILET FACILITIES.
 - ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) SHALL BE DETAINED AND PROPERLY TREATED OR DISPOSED.
 - SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL ON SITE. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS IS PROHIBITED.
 - RUBBISH, TRASH, GARBAGE, LITTER, OR OTHER SUCH MATERIALS SHALL BE DEPOSITED INTO SEALED CONTAINERS. MATERIALS SHALL BE PREVENTED FROM LEAVING THE PREMISES THROUGH THE ACTION OF WIND OR STORM WATER DISCHARGE INTO DRAINAGE DITCHES OR WATERS OF THE STATE.
 - ALL STORM WATER POLLUTION PREVENTION MEASURES PRESENTED ON THE PLAN, SHALL BE INITIATED AS SOON AS PRACTICABLE.

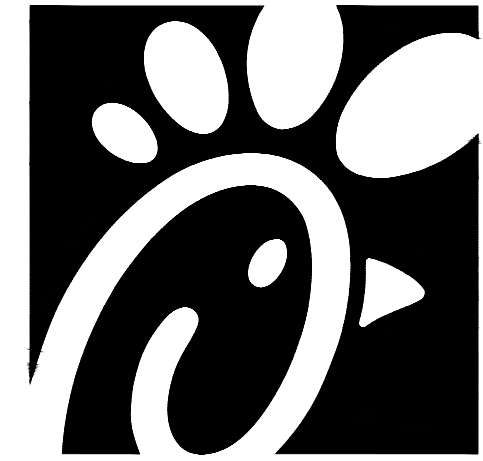
EROSION CONTROL NOTES (CONT.)

- STABILIZATION PRACTICES SHOULD BE INITIATED AS SOON AS PRACTICAL, BUT IN NO CASE MORE THAN 7 DAYS WHERE CONSTRUCTION HAS TEMPORARILY CEASED.
- DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY HAS PERMANENTLY STOPPED SHALL BE PERMANENTLY SEEDED. THESE AREAS SHALL BE SEEDED NO LATER THAN 7 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY OCCURRED IN THESE AREAS.
- IF THE ACTION OF VEHICLES TRAVELING OVER THE GRAVEL CONSTRUCTION ENTRANCES IS NOT SUFFICIENT TO REMOVE THE MAJORITY OF DIRT OR MUD, THEN THE TIRES MUST BE WASHED BEFORE THE VEHICLES ENTER A PUBLIC ROAD. IF WASHING IS USED, PROVISIONS MUST BE MADE TO INTERCEPT THE WASH WATER AND TRAP THE SEDIMENT BEFORE IT IS CARRIED OFF THE SITE.
- ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES ONTO ROADWAYS OR INTO STORM DRAINS MUST BE REMOVED AS SOON AS POSSIBLE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING SEDIMENT IN THE DETENTION POND AND ANY SEDIMENT THAT MAY HAVE COLLECTED IN THE STORM SEWER DRAINAGE SYSTEMS IN CONJUNCTION WITH THE STABILIZATION OF THE SITE.
- ON-SITE & OFF SITE SOIL STOCKPILE AND BORROW AREAS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION THROUGH IMPLEMENTATION OF BEST MANAGEMENT PRACTICES. STOCKPILE AND BORROW AREA LOCATIONS SHALL BE NOTED ON THE EROSION CONTROL PLAN AND PERMITTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS.
- SLOPES SHALL BE LEFT IN A ROUGHENED CONDITION DURING THE GRADING PHASE TO REDUCE RUNOFF VELOCITIES AND EROSION.
- DUE TO GRADE CHANGES DURING THE DEVELOPMENT OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE EROSION CONTROL MEASURES (SILT FENCES, ETC.) TO PREVENT EROSION.
- ALL CONSTRUCTION SHALL BE STABILIZED AT THE END OF EACH WORKING DAY. THIS INCLUDES BACK FILLING OF TRENCHES FOR UTILITY CONSTRUCTION AND PLACEMENT OF GRAVEL OR BITUMINOUS PAVING FOR ROAD CONSTRUCTION.

MAINTENANCE

- ALL MEASURES STATED ON THE EROSION AND SEDIMENT CONTROL PLAN, AND IN THE STORM WATER POLLUTION PREVENTION PLAN, SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE SITE. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY A QUALIFIED PERSON AT LEAST ONCE EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A 0.5" RAINFALL EVENT, AND CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:
- INLET PROTECTION DEVICES AND BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING, OR DETERIORATION.
 - ALL SEEDED AREAS SHALL BE CHECKED REGULARLY TO SEE THAT A GOOD STAND IS MAINTAINED. AREAS SHOULD BE FERTILIZED, WATERED AND RESEEDED AS NEEDED.
 - SILT FENCES SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED. SEDIMENT SHALL BE REMOVED FROM THE SILT FENCES WHEN IT REACHES ONE-HALF THE HEIGHT OF THE SILT FENCE.
 - THE CONSTRUCTION ENTRANCES SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING OF THE CONSTRUCTION ENTRANCES AS CONDITIONS DEMAND.
 - THE TEMPORARY PARKING AND STORAGE AREA SHALL BE KEPT IN GOOD CONDITION (SUITABLE FOR PARKING AND STORAGE). THIS MAY REQUIRE PERIODIC TOP DRESSING OF THE TEMPORARY PARKING AS CONDITIONS DEMAND.
 - OUTLET STRUCTURES IN THE SEDIMENTATION BASINS SHALL BE MAINTAINED IN OPERATIONAL CONDITIONS AT ALL TIMES. SEDIMENT SHALL BE REMOVED FROM SEDIMENT BASINS OR TRAPS WHEN THE DESIGN CAPACITY HAS BEEN REDUCED TO 55 CUBIC YARDS / ACRE.
 - ALL MAINTENANCE OPERATIONS SHALL BE DONE IN A TIMELY MANNER BUT IN NO CASE LATER THAN 2 CALENDAR DAYS FOLLOWING THE INSPECTION.

K:\CHS_LDEV\Chick-Fil-A\2216 - Macedonia FSU_Macedonia_OH\2023\CADD\PLAN SHEETS\C500 GENERAL NOTES.dwg
11 May 2023



Chick-fil-A
5200 Buffington Road
Atlanta, Georgia 30349-2998

Kimley»Horn

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CHICK-FIL-A
MACEDONIA FSU
8213 GOLDEN LINK BLVD
MACEDONIA, OH 44067

FSR#2216

REVISION SCHEDULE		
NO.	DATE	DESCRIPTION

CONSULTANT PROJECT #	268214000
PRINTED FOR	JHP
DATE	02/23/2023
DRAWN BY	AP

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SHEET
GENERAL NOTES

SHEET NUMBER

C500



May 18th, 2023

City of Macedonia
Building Department
9691 Valley View Road
Macedonia, OH 44056

RE: *Chick-Fil-A Macedonia FSU – Project Narrative*

Chick-Fil-A is proposing to reconfigure their parking layout and a portion of the approach of their drive-thru lanes to better accommodate traffic flow and queuing during peak hours of the drive-thru. This includes removing some parking stalls on the south side of the parking lot to provide a third queuing lane and reversing the directions of two existing one-way drive lanes.

The layout is proposed based on existing operations of the facility. Kimley-Horn also developed a Traffic Study to analyze the proposed layout to confirm the design approach.

Please contact me at (630) 487-3432 or jake.panter@kimley-horn.com should you have any questions.

Sincerely,

A handwritten signature in blue ink, appearing to read "Jake Panter".

Jake Panter

Macedonia CFA Site Reconfiguration

Traffic Operations Study

Macedonia, Ohio

May 2023

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INTRODUCTION

Kimley-Horn and Associates, Inc. (Kimley-Horn) was retained by Chick-Fil-A to perform a traffic study related to the reconfiguration of their restaurant in Macedonia, Ohio. The purpose of this study is to evaluate the operation of this reconfiguration relative to queuing of drive-through traffic and parking spaces. This site will retain its existing restaurant use and size. However, the drive-through lanes and site circulation pattern are being modified to improve traffic flow and queue storage. The site concept plan is provided in the **Appendix**.

As part of this study, Kimley-Horn collected traffic count data at the existing site to identify the AM, PM, and Lunch peak hour volumes. This report presents and documents Kimley-Horn's data collection, summarizes the evaluation of site circulation conditions, and outlines conclusions.

EXISTING CONDITIONS

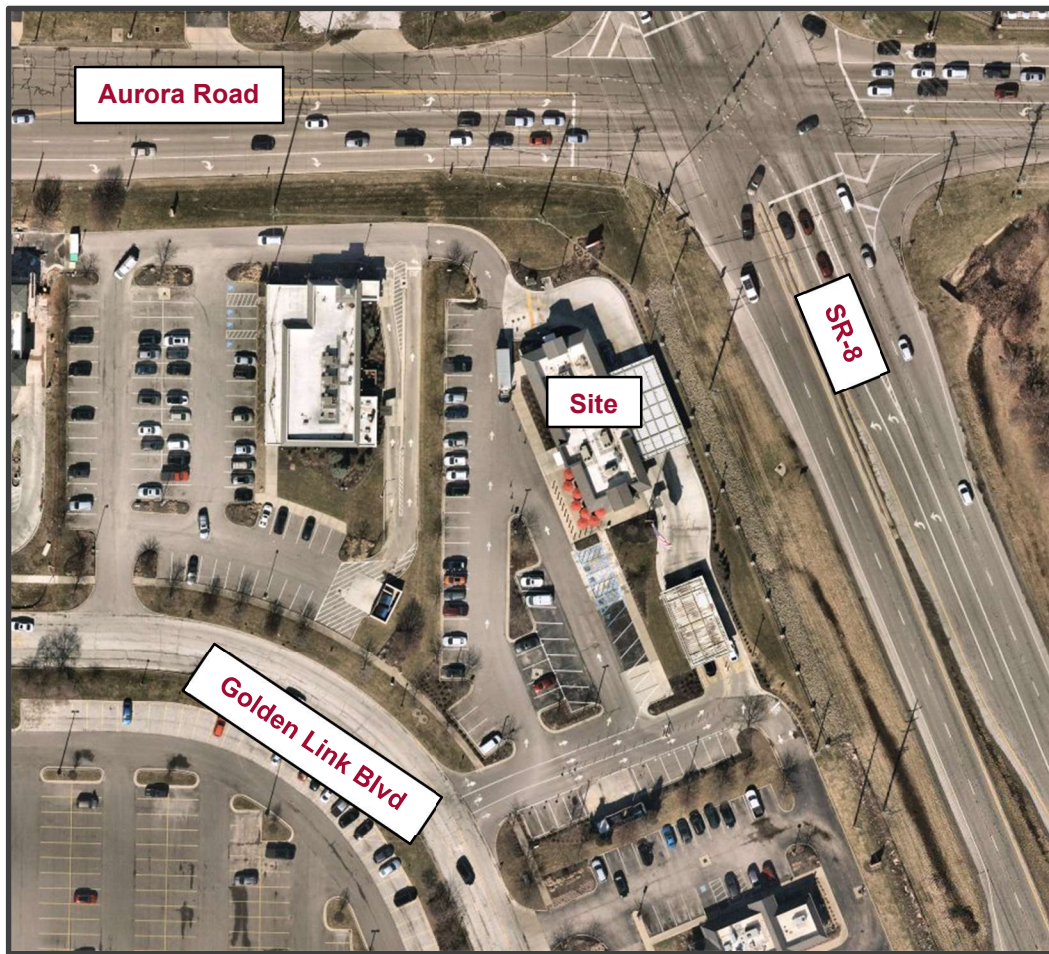
Kimley-Horn conducted a field visit to collect relevant information pertaining to the site, site access, the adjacent street system, current traffic volumes and operating conditions, lane configurations and traffic controls at nearby intersections, and other key characteristics. This section of the report details information on these existing conditions.

Existing Site Access Characteristics

The subject site is located within an existing mixed-use development south of Aurora Road and west of SR-8 in Macedonia, Ohio. The Chick-Fil-A has a single access point on Golden Link Boulevard that is currently entry-only and has connections to the adjacent commercial properties. Golden Link Boulevard is a three-lane section in the vicinity of the site and with a two-way left turn lane (TWLTL) that provides storage space for left turn vehicles entering the site.

The entry-only site access point provides three entering lanes, with two routed to the drive-through on the east side of the building and one additional lane routed to the parking area on the west side of the building. Figure 1 shows the existing site layout. In general, the physical site characteristics such as the location of the building and the paved area are to remain the same with modifications made to striping, minor island/pavement revisions, and operations to better facilitate ingress, egress, and queue storage.

Figure 1: Existing Site Conditions



Traffic Count Data Collection

Turning movement counts for the study intersections were obtained via MioVision traffic cameras. The traffic camera was placed on March 21st, 2023, at the site access point on Golden Link Boulevard. The traffic counts were performed for 24-hours and included the weekday morning, lunch, and evening peak periods, coinciding with the peak hours of traffic activity on the adjacent street. The existing peak hour traffic volumes are provided below in **Exhibit 1** and **Exhibit 2** and in the **Appendix**.

Exhibit 1: Existing 2023 Counts AM & PM Peak Hour Vehicle Volumes



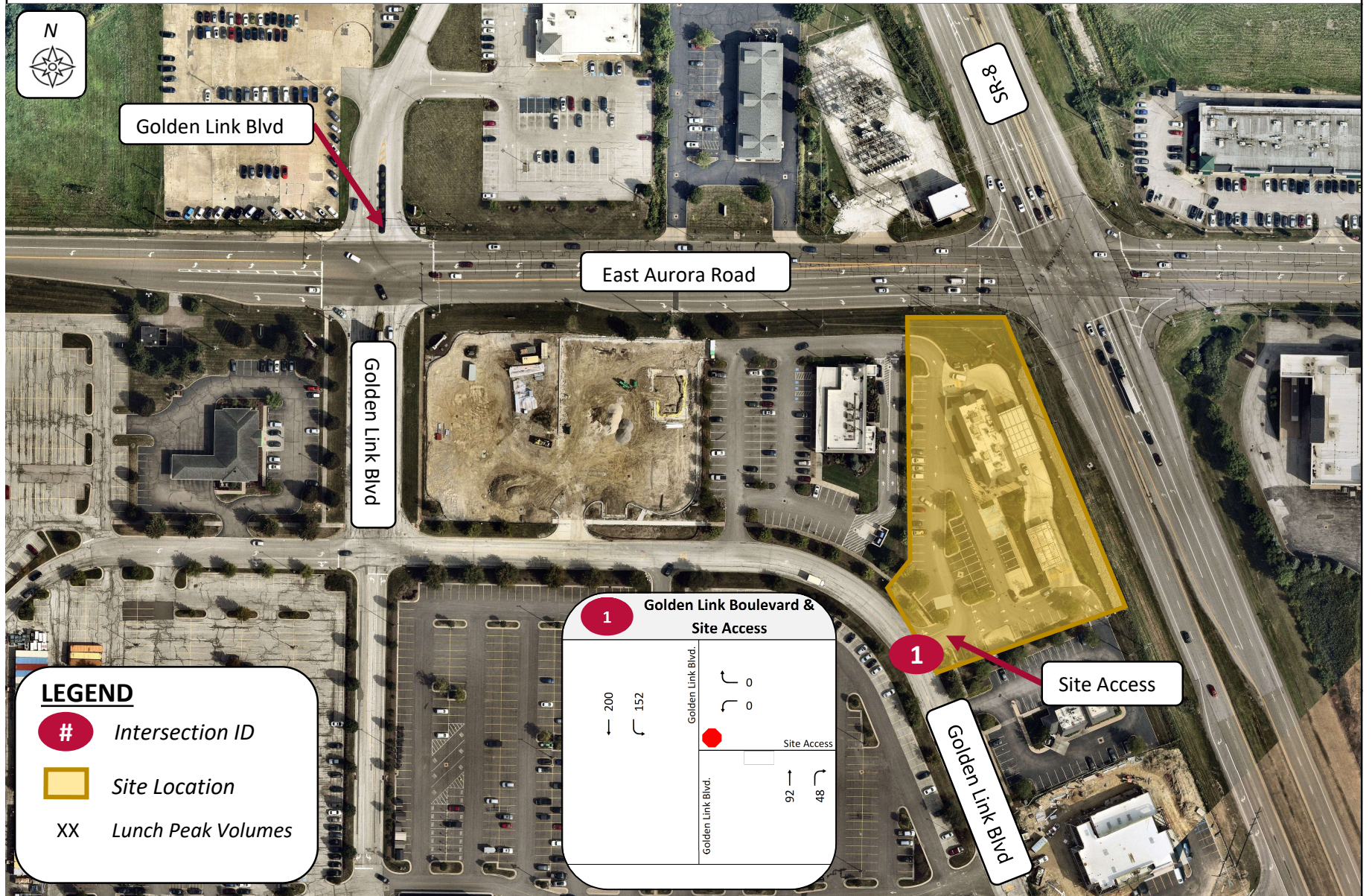
LEGEND

Intersection ID

Yellow Box Site Location

XX (XX) AM (PM) Peak Hour Vehicle Volumes

Exhibit 2: Existing 2023 Counts Lunch Peak Volumes



PROPOSED CONDITIONS

As noted above, the site size and general characteristics will remain the same in the proposed. Modifications are proposed to the parking lot to remove the existing parking along the south side of the lot to provide additional queue storage.

In the proposed condition, the southern cross access shared with the parcel to the south will be closed to provide space for a partial third queue storage lane for drive-through vehicles. Additionally, the temporary entry-only condition at the access along Golden Link Boulevard will be revised to permit egress with a four-lane driveway providing two inbound and two outbound lanes. Vehicles exiting the drive-through at the north end of the site will still be directed to leave the site via the shared access point with the property to the west. The proposed conditions are illustrated on the conceptual site plan included in the **Appendix**.

ANALYSIS

Kimley-Horn completed a site circulation analysis for the subject site to analyze the proposed configuration. This section details the site demand and the associated queue lengths and storage required on the site.

Site Demand Analysis

The collected count data was evaluated to identify the peak hours to determine the peak site demand and amount of queueing storage required. Kimley-Horn reviewed the collected count data and determined that the highest demand on the site occurred during the lunch peak (11:45 AM – 12:45 PM). Additionally, Kimley-Horn reviewed available site operations data to determine the average total on-site time for a vehicle from the time it enters the drive-through queue to the time that it exits the site after an order is processed.

The restaurant is comprised of both dine-in and drive-through options for serving patrons. A large portion of the arriving patrons, approximately 83%, utilize the drive-through with approximately 17% of patrons utilizing the indoor ordering/seating option.

Table 1 below shows the volumes and calculated results used to determine the anticipated queue for the peak demand period. The total entering volume was obtained from the count data for 1-hour, 15-minute, and 5-minute peaks to evaluate anticipated drive-through queue lengths under multiple demand intensities.

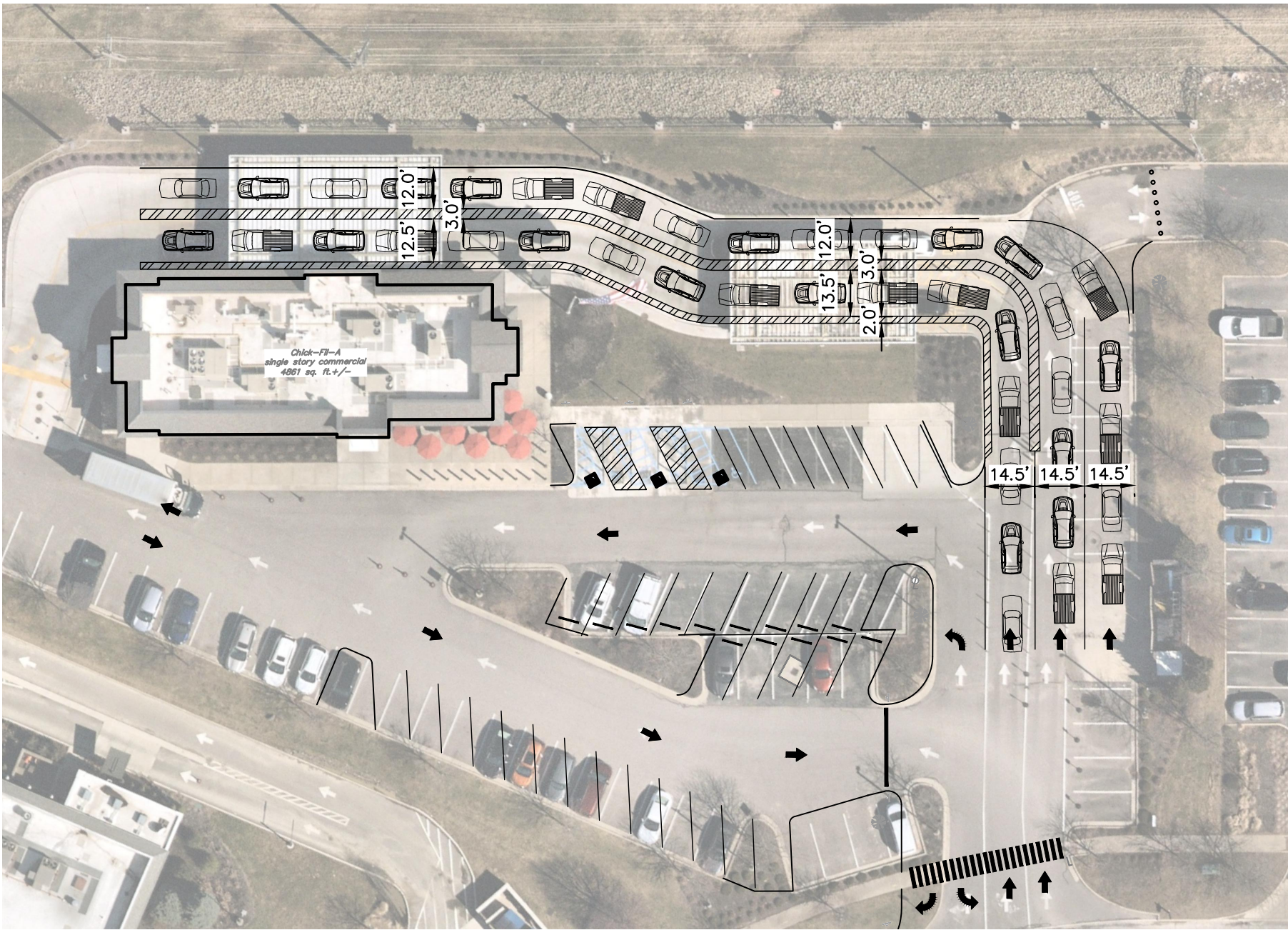
Table 1: Summary of Site Demand Data

<i>Period</i>	<i>Total Entering</i>	<i>Drive-Thru</i>
<i>1-hour</i>	200	166
<i>15-minutes</i>	69	57
<i>5-minutes</i>	29	24

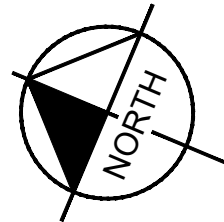
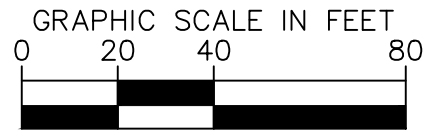
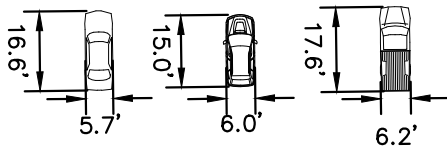
Queue Storage Analysis

Currently the existing site configuration provides approximately 750 feet of storage length available for the drive-through that can accommodate approximately 35 queued vehicles. The proposed site plan provides an additional 100 feet of space to store vehicles that is room for potentially 5 more queued vehicles. Exhibit 3 shows the drive through stacking including the additional 100 feet of storage space being provided by the proposed reconfiguration.

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NOTES
TOTAL VEHICLE COUNT: 40



*PRELIMINARY - NOT FOR CONSTRUCTION

CHICK-FIL-A DRIVE-THROUGH STACKING EXHIBIT

EX.

8213 GOLDEN LINK BLVD, MACEDONIA, OH 44067

The 750 feet of storage space provides room for approximately 35 vehicles assuming a typical 20 feet requirement per vehicle. Based on the calculations provided above, it is anticipated that the site can accommodate the typical demand with space for five additional vehicles in the peak hour period.

Table 1 shows that the peak 5-minute demand on the drive-through is 24 vehicles. Assuming a worst-case scenario where these vehicles all arrive before the first vehicle is processed, this would result in a queue of 24 vehicles with the available area that can queue 40 vehicles. This indicates that the available queuing area for the drive-through can accommodate 16 more vehicles than the maximum noon peak as counted.

Parking Analysis

The Chick-Fil-A site will provide 45 parking spaces (plus 3 ADA spaces) upon implementation of proposed improvements. Using the hourly volume total shown in **Table 1**, 34 patrons (17%) are anticipated to utilize the indoor ordering/seating area during the peak hour. Assuming a dine in time of 30 minutes per patron, approximately 17 parking spaces are anticipated to be utilized by customers during peak hour conditions. This leaves 28 parking spaces open to accommodate staff parking. Due to the nature of the fast-food restaurant, the 30-min average time for “order-in” customers provides a conservative estimate, as a percentage of patrons are expected to order inside and will not dine-in the restaurant resulting in a shorter time on site. Based on the stated assumptions, the proposed site layout is anticipated to provide an adequate number of parking spaces to accommodate the peak hour demand.

CONCLUSIONS

The proposed reconfiguration of the queuing and parking areas of the Chick-Fil-A restaurant will provide additional space for the queuing of vehicles using the drive-through at this location. This change represents an increase of queuing area for 5 additional vehicles which should accommodate approximately 40 vehicles. This is roughly 60 percent more than the maximum queue of 24 vehicles as determined from the collected data.

This analysis and the resulting recommendations are based on typical weekday traffic patterns. Special events, holidays, or other infrequent occurrences of higher demand could impact the results of this analysis and cause the site to experience longer queue lengths in the drive-through area. It is recommended that on site staff monitor the queue lengths and provide the extra staff and resources necessary to accommodate atypical demand on the site. Additionally, these queue lengths were calculated based on an estimated processing time which could impact results.

While there are atypical circumstances that could result in longer queue lengths, in general the proposed improvements are a benefit to the site. Additional queue storage space is provided by means of removing parking along the south side of the site and removing a potential conflict point by closing the southernmost share access with the parcel to the south. The access point along Golden Link Boulevard is also proposed to be modified to allow ingress and egress. This modification provides a configuration that drivers are more accustomed to encountering and eliminates accidental “wrong-way” use. Additionally, the number of proposed parking spaces is anticipated to provide suitable space during peak conditions. Overall, the site reconfiguration is a net benefit to the site operations and to the surrounding roadway network.

APPENDIX

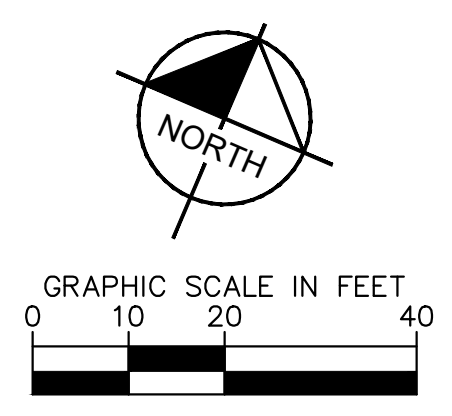
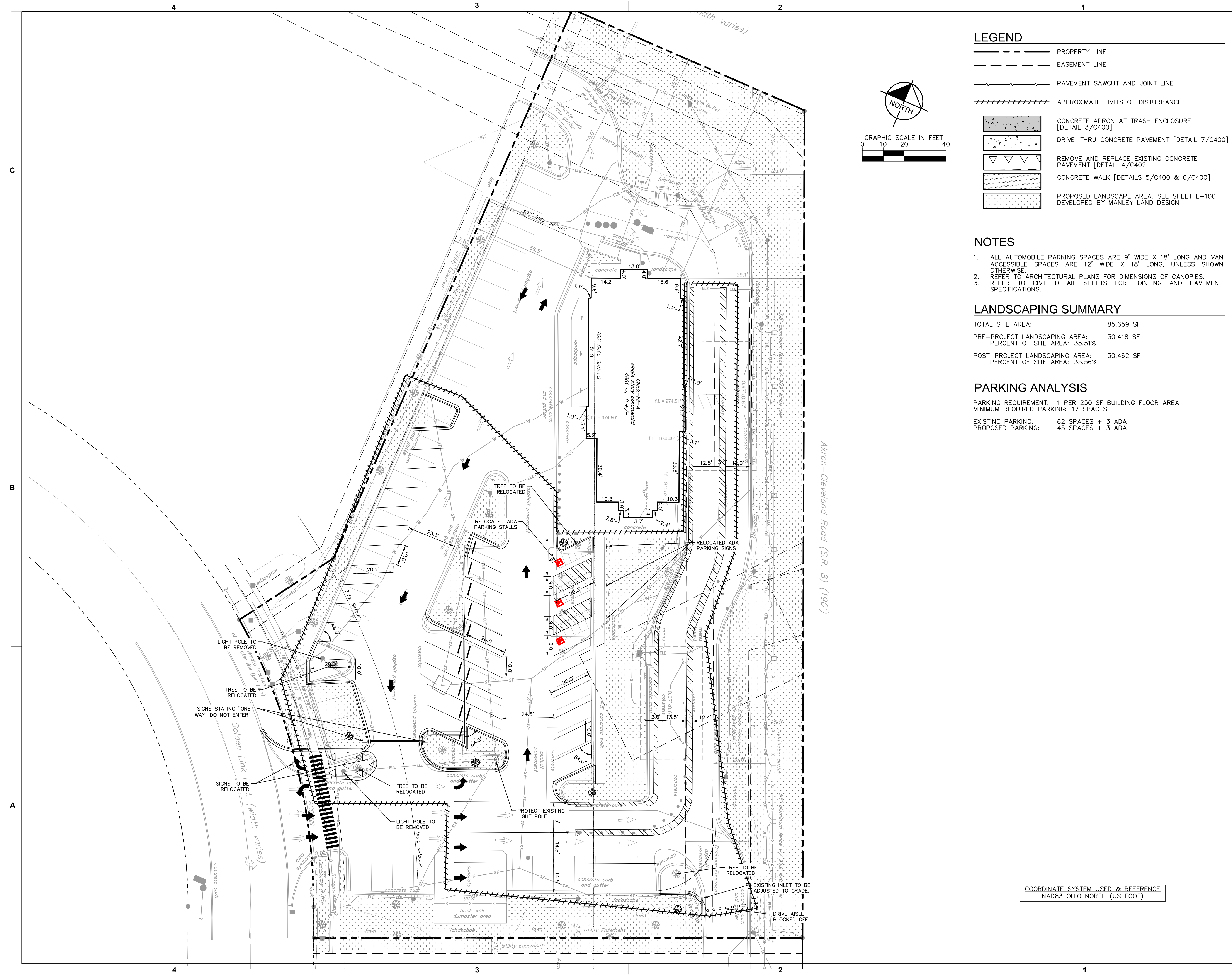
A – Conceptual Site Plan

B – Traffic Count Data

Appendix A

Conceptual Site Plan

Kimley»»Horn



LEGEND

- PROPERTY LINE
- EASEMENT LINE
- PAVEMENT SAWCUT AND JOINT LINE
- APPROXIMATE LIMITS OF DISTURBANCE
- CONCRETE APRON AT TRASH ENCLOSURE [DETAIL 3/C400]
- DRIVE-THRU CONCRETE PAVEMENT [DETAIL 7/C400]
- REMOVE AND REPLACE EXISTING CONCRETE PAVEMENT [DETAIL 4/C402]
- CONCRETE WALK [DETAILS 5/C400 & 6/C400]
- PROPOSED LANDSCAPE AREA. SEE SHEET L-100 DEVELOPED BY MANLEY LAND DESIGN

- NOTES**
- ALL AUTOMOBILE PARKING SPACES ARE 9' WIDE X 18' LONG AND VAN ACCESSIBLE SPACES ARE 12' WIDE X 18' LONG, UNLESS SHOWN OTHERWISE.
 - REFER TO ARCHITECTURAL PLANS FOR DIMENSIONS OF CANOPIES.
 - REFER TO CIVIL DETAIL SHEETS FOR JOINTING AND PAVEMENT SPECIFICATIONS.

LANDSCAPING SUMMARY

TOTAL SITE AREA:	85,659 SF
PRE-PROJECT LANDSCAPING AREA:	30,418 SF
PERCENT OF SITE AREA:	35.51%
POST-PROJECT LANDSCAPING AREA:	30,462 SF
PERCENT OF SITE AREA:	35.56%

PARKING ANALYSIS

PARKING REQUIREMENT:	1 PER 250 SF BUILDING FLOOR AREA
MINIMUM REQUIRED PARKING:	17 SPACES
EXISTING PARKING:	62 SPACES + 3 ADA
PROPOSED PARKING:	45 SPACES + 3 ADA

COORDINATE SYSTEM USED & REFERENCE
NAD83 OHIO NORTH (US FOOT)



Chick-fil-A

Chick-fil-A
5200 Buffington Road
Atlanta, Georgia 30349-2998

Kimley»Horn

© 2023 KIMLEY-HORN AND ASSOCIATES, INC.
111 WEST JACKSON BLVD., SUITE 1320, CHICAGO, IL 60604
PHONE: 312-726-9445
WWW.KIMLEY-HORN.COM

CHICK-FIL-A
MACEDONIA FSU
8213 GOLDEN LINK BLVD
MACEDONIA, OH 44067
FSR#2216

REVISION SCHEDULE

NO.	DATE	DESCRIPTION
-----	------	-------------

CONSULTANT PROJECT #	268214000
PRINTED FOR	JHP
DATE	02/23/2023
DRAWN BY	AP

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SHEET NUMBER

Appendix B

Count Data

02_Site Access - TMC

Tue Mar 21, 2023

Full Length (1 PM-1 PM (+1))

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				Int
	L	R	U	App	T	R	U	App	L	T	U	App	
2023-03-21 1:00PM	0	0	0	0	7	2	0	9	11	4	0	15	24
1:05PM	0	0	0	0	5	2	0	7	7	11	0	18	25
1:10PM	0	0	0	0	2	1	0	3	6	15	0	21	24
1:15PM	0	0	0	0	8	2	0	10	6	12	0	18	28
1:20PM	0	0	0	0	4	3	0	7	10	11	0	21	28
1:25PM	0	0	0	0	5	3	0	8	5	3	0	8	16
1:30PM	0	0	0	0	4	4	0	8	8	7	0	15	23
1:35PM	0	0	0	0	5	2	0	7	6	10	0	16	23
1:40PM	0	0	0	0	9	1	0	10	9	10	0	19	29
1:45PM	0	1	0	1	5	2	0	7	5	8	0	13	21
1:50PM	1	0	0	1	5	2	0	7	10	5	0	15	23
1:55PM	0	0	0	0	3	1	0	4	7	11	0	18	22
Hourly Total	1	1	0	2	62	25	0	87	90	107	0	197	286
2:00PM	1	0	0	1	9	2	0	11	5	8	0	13	25
2:05PM	0	0	0	0	6	1	0	7	3	10	0	13	20
2:10PM	0	0	0	0	5	0	0	5	5	12	0	17	22
2:15PM	0	0	0	0	10	3	0	13	2	6	0	8	21
2:20PM	0	0	0	0	6	6	0	12	8	11	0	19	31
2:25PM	0	0	0	0	7	1	0	8	8	10	0	18	26
2:30PM	0	0	0	0	4	1	0	5	3	6	0	9	14
2:35PM	0	0	0	0	2	1	0	3	9	8	0	17	20
2:40PM	0	0	0	0	7	3	0	10	6	10	0	16	26
2:45PM	0	0	0	0	4	3	0	7	6	8	0	14	21
2:50PM	1	0	0	1	0	3	0	3	9	11	0	20	24
2:55PM	0	0	0	0	3	2	0	5	4	9	0	13	18
Hourly Total	2	0	0	2	63	26	0	89	68	109	0	177	268
3:00PM	0	1	0	1	8	0	0	8	2	9	0	11	20
3:05PM	0	0	0	0	4	1	0	5	2	4	0	6	11
3:10PM	0	1	0	1	3	1	0	4	6	8	0	14	19
3:15PM	0	0	0	0	8	4	0	12	4	5	0	9	21
3:20PM	0	0	0	0	4	1	0	5	7	10	0	17	22
3:25PM	0	0	0	0	1	2	0	3	5	9	1	15	18
3:30PM	0	0	0	0	5	3	0	8	5	9	0	14	22
3:35PM	0	0	0	0	6	1	0	7	4	12	0	16	23
3:40PM	0	0	0	0	7	2	0	9	5	7	0	12	21
3:45PM	0	0	0	0	7	2	0	9	4	7	0	11	20
3:50PM	0	0	0	0	4	3	0	7	12	7	0	19	26
3:55PM	0	0	0	0	2	0	0	2	5	7	0	12	14
Hourly Total	0	2	0	2	59	20	0	79	61	94	1	156	237
4:00PM	0	0	0	0	6	1	0	7	3	17	0	20	27
4:05PM	0	1	0	1	4	2	0	6	6	17	0	23	30
4:10PM	0	0	0	0	6	1	0	7	8	4	0	12	19
4:15PM	0	1	0	1	6	0	0	6	7	7	0	14	21
4:20PM	0	0	0	0	2	1	0	3	10	6	0	16	19
4:25PM	0	0	0	0	7	0	0	7	5	11	0	16	23
4:30PM	0	1	0	1	5	0	0	5	5	9	0	14	20
4:35PM	0	0	0	0	6	3	0	9	8	9	0	17	26
4:40PM	0	0	0	0	6	4	0	10	7	10	0	17	27
4:45PM	0	0	0	0	1	2	0	3	5	9	0	14	17
4:50PM	0	0	0	0	5	3	0	8	6	13	0	19	27
4:55PM	0	0	0	0	7	3	0	10	10	13	0	23	33
Hourly Total	0	3	0	3	61	20	0	81	80	125	0	205	289
5:00PM	0	0	0	0	5	4	0	9	7	14	0	21	30
5:05PM	0	0	0	0	6	1	0	7	5	12	0	17	24

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				
Time	L	R	U	App	T	R	U	App	L	T	U	App	Int
5:10PM	0	0	0	0	8	3	0	11	8	20	0	28	39
5:15PM	0	0	0	0	7	3	0	10	7	14	0	21	31
5:20PM	0	0	0	0	4	3	0	7	7	12	0	19	26
5:25PM	0	0	0	0	5	6	0	11	8	16	0	24	35
5:30PM	0	1	0	1	7	1	0	8	4	13	0	17	26
5:35PM	0	0	0	0	3	2	0	5	7	13	0	20	25
5:40PM	0	0	0	0	5	2	1	8	10	12	0	22	30
5:45PM	0	0	0	0	6	2	0	8	6	19	0	25	33
5:50PM	0	0	0	0	5	2	0	7	5	9	0	14	21
5:55PM	0	1	0	1	8	5	0	13	4	10	0	14	28
Hourly Total	0	2	0	2	69	34	1	104	78	164	0	242	348
6:00PM	0	0	0	0	4	2	0	6	8	16	0	24	30
6:05PM	0	0	0	0	7	2	0	9	9	12	0	21	30
6:10PM	0	0	0	0	7	0	0	7	10	16	0	26	33
6:15PM	0	0	0	0	7	5	0	12	8	17	0	25	37
6:20PM	0	0	0	0	4	0	0	4	9	14	0	23	27
6:25PM	0	0	0	0	8	1	0	9	4	7	0	11	20
6:30PM	0	0	0	0	5	4	0	9	10	14	0	24	33
6:35PM	0	0	0	0	6	4	0	10	8	10	0	18	28
6:40PM	0	0	0	0	3	1	0	4	10	8	0	18	22
6:45PM	0	0	0	0	5	2	0	7	8	11	0	19	26
6:50PM	0	0	0	0	6	3	0	9	4	10	0	14	23
6:55PM	0	0	0	0	8	3	0	11	5	11	0	16	27
Hourly Total	0	0	0	0	70	27	0	97	93	146	0	239	336
7:00PM	0	0	0	0	5	3	0	8	6	20	0	26	34
7:05PM	0	0	0	0	10	1	0	11	6	11	1	18	29
7:10PM	0	0	0	0	9	3	0	12	7	14	0	21	33
7:15PM	0	0	0	0	9	4	0	13	8	11	0	19	32
7:20PM	0	1	0	1	8	0	0	8	5	11	0	16	25
7:25PM	0	0	0	0	8	5	0	13	5	7	0	12	25
7:30PM	0	0	0	0	11	1	0	12	7	12	0	19	31
7:35PM	1	0	0	1	12	1	0	13	12	10	0	22	36
7:40PM	0	0	0	0	3	4	0	7	9	4	0	13	20
7:45PM	0	0	0	0	9	1	0	10	2	12	0	14	24
7:50PM	0	0	0	0	5	2	0	7	5	8	0	13	20
7:55PM	0	0	0	0	7	0	0	7	9	11	0	20	27
Hourly Total	1	1	0	2	96	25	0	121	81	131	1	213	336
8:00PM	0	0	0	0	8	3	0	11	5	7	0	12	23
8:05PM	0	0	0	0	7	2	0	9	8	11	0	19	28
8:10PM	0	1	0	1	3	5	0	8	9	6	0	15	24
8:15PM	0	0	0	0	5	0	0	5	4	13	0	17	22
8:20PM	0	0	0	0	1	0	0	1	3	7	0	10	11
8:25PM	0	0	0	0	6	3	0	9	3	6	0	9	18
8:30PM	0	1	0	1	3	1	0	4	4	5	0	9	14
8:35PM	0	0	0	0	6	3	0	9	4	4	0	8	17
8:40PM	0	0	0	0	4	1	0	5	6	6	0	12	17
8:45PM	0	0	0	0	4	1	0	5	3	5	0	8	13
8:50PM	0	0	0	0	4	1	0	5	5	5	0	10	15
8:55PM	0	0	0	0	4	1	0	5	6	8	0	14	19
Hourly Total	0	2	0	2	55	21	0	76	60	83	0	143	221
9:00PM	0	0	0	0	2	0	0	2	3	5	0	8	10
9:05PM	0	0	0	0	5	1	0	6	3	3	0	6	12
9:10PM	0	0	0	0	3	0	0	3	3	7	0	10	13
9:15PM	0	0	0	0	1	2	0	3	5	8	0	13	16
9:20PM	0	0	0	0	2	1	0	3	2	6	0	8	11
9:25PM	0	0	0	0	6	1	1	8	0	4	0	4	12
9:30PM	0	0	0	0	2	2	0	4	3	1	0	4	8
9:35PM	0	0	0	0	4	0	0	4	2	3	0	5	9
9:40PM	0	0	0	0	3	2	0	5	1	3	0	4	9
9:45PM	0	0	0	0	1	0	0	1	1	3	0	4	5

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				
Time	L	R	U	App	T	R	U	App	L	T	U	App	Int
9:50PM	0	0	0	0	3	0	0	3	1	3	0	4	7
9:55PM	0	0	0	0	0	3	0	3	0	2	0	2	5
Hourly Total	0	0	0	0	32	12	1	45	24	48	0	72	117
10:00PM	0	1	0	1	1	0	0	1	0	0	0	0	2
10:05PM	1	0	0	1	0	0	0	0	0	3	0	3	4
10:10PM	0	0	0	0	0	0	0	0	0	0	0	0	0
10:15PM	0	0	0	0	0	0	0	0	0	0	0	0	0
10:20PM	0	0	0	0	1	0	0	1	0	0	0	0	1
10:25PM	0	0	0	0	0	0	0	0	0	2	0	2	2
10:30PM	0	0	0	0	1	0	0	1	1	0	0	1	2
10:35PM	0	0	0	0	1	0	0	1	0	0	0	0	1
10:40PM	0	0	0	0	0	0	0	0	0	0	0	0	0
10:45PM	0	1	0	1	1	0	0	1	0	0	0	0	2
10:50PM	1	0	0	1	1	0	0	1	0	0	0	0	2
10:55PM	0	4	0	4	1	0	0	1	0	0	0	0	5
Hourly Total	2	6	0	8	7	0	0	7	1	5	0	6	21
11:00PM	0	0	0	0	0	0	0	0	0	0	0	0	0
11:05PM	0	1	0	1	2	0	0	2	0	0	0	0	3
11:10PM	0	0	0	0	0	0	0	0	0	0	0	0	0
11:15PM	0	0	0	0	0	0	0	0	0	0	0	0	0
11:20PM	0	0	0	0	0	0	0	0	0	0	0	0	0
11:25PM	1	0	0	1	0	0	0	0	0	0	0	0	1
11:30PM	0	1	0	1	0	0	0	0	0	0	0	0	1
11:35PM	0	0	0	0	0	0	0	0	0	0	0	0	0
11:40PM	0	0	0	0	0	0	0	0	0	0	0	0	0
11:45PM	0	0	0	0	0	0	0	0	0	0	0	0	0
11:50PM	0	0	0	0	3	0	0	3	0	0	0	0	3
11:55PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Hourly Total	1	2	0	3	5	0	0	5	0	0	0	0	8
2023-03-22 12:00AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:05AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:10AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:15AM	0	0	0	0	0	1	0	1	0	0	0	0	1
12:20AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:25AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:35AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:40AM	1	0	0	1	0	0	0	0	0	0	0	0	1
12:45AM	0	0	0	0	0	0	0	0	0	0	0	0	0
12:50AM	0	0	0	0	1	0	0	1	0	0	0	0	1
12:55AM	0	0	0	0	0	0	0	0	0	0	0	0	0
Hourly Total	1	0	0	1	1	1	0	2	0	0	0	0	3
1:00AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:05AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:10AM	0	1	0	1	0	0	0	0	0	0	0	0	1
1:15AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:20AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:25AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:30AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:35AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:40AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:45AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:50AM	0	0	0	0	0	0	0	0	0	0	0	0	0
1:55AM	0	0	0	0	0	0	0	0	0	0	0	0	0
Hourly Total	0	1	0	1	0	0	0	0	0	0	0	0	1
2:00AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:05AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:10AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:15AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:20AM	0	0	0	0	0	0	0	0	0	0	0	0	0

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				
Time	L	R	U	App	T	R	U	App	L	T	U	App	Int
2:25AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:30AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:35AM	0	0	0	0	0	0	0	0	0	1	0	1	1
2:40AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:45AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:50AM	0	0	0	0	0	0	0	0	0	0	0	0	0
2:55AM	0	0	0	0	0	0	0	0	0	0	0	0	0
Hourly Total	0	0	0	0	0	0	0	0	0	1	0	1	1
3:00AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:05AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:10AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:15AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:20AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:25AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:30AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:35AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:40AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:45AM	0	0	0	0	0	0	0	0	0	1	0	1	1
3:50AM	0	0	0	0	0	0	0	0	0	0	0	0	0
3:55AM	0	0	0	0	0	0	0	0	0	0	0	0	0
Hourly Total	0	0	0	0	0	0	0	0	0	1	0	1	1
4:00AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:05AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:10AM	0	0	0	0	0	0	0	0	0	1	0	1	1
4:15AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:20AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:25AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30AM	1	0	0	1	0	0	0	0	0	0	0	0	1
4:35AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:40AM	0	0	0	0	0	0	0	0	2	0	0	2	2
4:45AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:50AM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:55AM	0	0	0	0	0	0	0	0	0	0	0	0	0
Hourly Total	1	0	0	1	0	0	0	0	2	1	0	3	4
5:00AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:05AM	0	0	0	0	0	0	0	0	0	1	0	1	1
5:10AM	0	0	0	0	2	0	0	2	0	1	0	1	3
5:15AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:20AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:25AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:35AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:40AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:50AM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:55AM	0	0	0	0	0	1	0	1	3	1	0	4	5
Hourly Total	0	0	0	0	2	1	0	3	3	3	0	6	9
6:00AM	0	0	0	0	0	0	0	0	1	2	0	3	3
6:05AM	0	0	0	0	0	0	0	0	1	0	0	1	1
6:10AM	0	0	0	0	0	0	0	0	1	0	0	1	1
6:15AM	0	0	0	0	0	0	0	0	0	0	0	0	0
6:20AM	1	0	0	1	0	0	0	0	1	1	0	2	3
6:25AM	0	0	0	0	1	0	0	1	0	2	0	2	3
6:30AM	0	0	0	0	0	0	1	1	1	2	0	3	4
6:35AM	0	0	0	0	0	0	0	0	3	2	0	5	5
6:40AM	0	1	0	1	0	0	0	0	1	0	0	1	2
6:45AM	0	1	0	1	0	3	0	3	0	2	0	2	6
6:50AM	0	0	0	0	0	0	0	0	5	0	0	5	5
6:55AM	0	0	0	0	1	1	0	2	1	1	0	2	4
Hourly Total	1	2	0	3	2	4	1	7	15	12	0	27	37

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				
Time	L	R	U	App	T	R	U	App	L	T	U	App	Int
7:00AM	0	0	0	0	1	0	0	1	5	2	0	7	8
7:05AM	0	0	0	0	0	1	0	1	0	2	0	2	3
7:10AM	0	0	0	0	1	0	0	1	0	3	0	3	4
7:15AM	0	0	0	0	1	0	0	1	6	0	0	6	7
7:20AM	0	0	0	0	0	1	0	1	3	4	0	7	8
7:25AM	0	0	0	0	1	0	0	1	1	2	0	3	4
7:30AM	0	0	0	0	1	0	0	1	2	3	0	5	6
7:35AM	0	0	0	0	2	0	0	2	2	2	0	4	6
7:40AM	0	0	0	0	0	0	0	0	2	1	0	3	3
7:45AM	0	0	0	0	1	0	0	1	2	3	0	5	6
7:50AM	0	0	0	0	2	1	0	3	2	2	0	4	7
7:55AM	0	0	0	0	0	0	0	0	2	1	0	3	3
Hourly Total	0	0	0	0	10	3	0	13	27	25	0	52	65
8:00AM	0	0	0	0	1	0	0	1	1	1	0	2	3
8:05AM	0	0	0	0	1	1	0	2	5	1	0	6	8
8:10AM	0	0	0	0	0	0	0	0	0	1	0	1	1
8:15AM	0	1	0	1	1	2	0	3	3	0	0	3	7
8:20AM	0	0	0	0	2	3	0	5	2	2	0	4	9
8:25AM	0	1	0	1	2	1	0	3	2	5	0	7	11
8:30AM	0	1	0	1	1	0	0	1	4	9	0	13	15
8:35AM	0	0	0	0	1	0	0	1	1	1	0	2	3
8:40AM	0	0	0	0	1	1	0	2	1	3	0	4	6
8:45AM	0	0	0	0	0	3	0	3	2	4	0	6	9
8:50AM	0	0	0	0	2	0	0	2	4	1	0	5	7
8:55AM	0	0	0	0	2	0	0	2	2	4	0	6	8
Hourly Total	0	3	0	3	14	11	0	25	27	32	0	59	87
9:00AM	0	0	0	0	0	1	0	1	2	2	0	4	5
9:05AM	0	0	0	0	2	0	0	2	2	1	0	3	5
9:10AM	0	0	0	0	0	2	0	2	4	2	1	7	9
9:15AM	0	1	0	1	1	3	0	4	3	4	0	7	12
9:20AM	0	0	0	0	1	1	0	2	1	5	0	6	8
9:25AM	0	0	0	0	3	2	0	5	5	4	0	9	14
9:30AM	0	0	0	0	2	1	0	3	0	8	0	8	11
9:35AM	0	0	0	0	0	4	0	4	1	1	0	2	6
9:40AM	0	0	0	0	0	2	0	2	3	5	0	8	10
9:45AM	0	1	0	1	1	1	0	2	5	4	0	9	12
9:50AM	0	0	0	0	3	1	0	4	2	5	0	7	11
9:55AM	0	0	0	0	1	0	0	1	2	3	0	5	6
Hourly Total	0	2	0	2	14	18	0	32	30	44	1	75	109
10:00AM	0	0	0	0	0	1	0	1	1	8	0	9	10
10:05AM	0	0	0	0	0	0	0	0	2	2	0	4	4
10:10AM	0	0	0	0	0	0	0	0	3	4	0	7	7
10:15AM	0	0	0	0	1	5	0	6	3	5	0	8	14
10:20AM	0	0	0	0	1	1	0	2	5	3	0	8	10
10:25AM	0	0	0	0	1	2	0	3	4	0	0	4	7
10:30AM	0	0	0	0	0	0	0	0	6	6	0	12	12
10:35AM	0	0	0	0	3	3	0	6	8	5	0	13	19
10:40AM	0	0	0	0	5	1	0	6	5	3	0	8	14
10:45AM	0	0	0	0	4	3	0	7	8	4	0	12	19
10:50AM	0	0	0	0	1	1	0	2	12	7	0	19	21
10:55AM	0	0	0	0	3	0	0	3	8	10	0	18	21
Hourly Total	0	0	0	0	19	17	0	36	65	57	0	122	158
11:00AM	1	0	0	1	6	3	0	9	5	11	0	16	26
11:05AM	0	0	0	0	3	4	0	7	5	9	0	14	21
11:10AM	0	1	0	1	4	1	0	5	8	13	0	21	27
11:15AM	0	0	0	0	7	2	0	9	11	5	0	16	25
11:20AM	0	0	0	0	2	3	0	5	3	16	0	19	24
11:25AM	0	0	0	0	6	1	0	7	10	8	0	18	25
11:30AM	0	0	0	0	4	1	0	5	5	14	0	19	24
11:35AM	0	0	0	0	9	3	0	12	8	17	0	25	37

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				Int
	L	R	U	App	T	R	U	App	L	T	U	App	
11:40AM	0	0	0	0	5	3	0	8	9	12	0	21	29
11:45AM	0	0	0	0	7	1	0	8	10	18	0	28	36
11:50AM	0	0	0	0	6	5	0	11	14	12	0	26	37
11:55AM	0	0	0	0	5	2	0	7	9	23	0	32	39
Hourly Total	1	1	0	2	64	29	0	93	97	158	0	255	350
12:00PM	0	0	0	0	9	3	0	12	18	18	0	36	48
12:05PM	1	0	0	1	5	7	0	12	22	17	0	39	52
12:10PM	0	0	0	0	5	4	0	9	13	17	0	30	39
12:15PM	0	0	0	0	15	5	0	20	13	16	1	30	50
12:20PM	0	0	0	0	12	7	0	19	8	20	0	28	47
12:25PM	0	0	0	0	9	5	0	14	10	11	0	21	35
12:30PM	1	0	0	1	5	2	0	7	10	18	0	28	36
12:35PM	0	0	0	0	5	5	0	10	11	13	0	24	34
12:40PM	0	0	0	0	9	2	0	11	14	17	0	31	42
12:45PM	0	1	0	1	6	3	0	9	4	15	0	19	29
12:50PM	0	0	0	0	4	3	0	7	8	18	0	26	33
12:55PM	0	0	0	0	3	0	0	3	7	7	0	14	17
Hourly Total	2	1	0	3	87	46	0	133	138	187	1	326	462
Total	13	29	0	42	792	340	3	1135	1040	1533	4	2577	3754
% Approach	31.0%	69.0%	0%	-	69.8%	30.0%	0.3%	-	40.4%	59.5%	0.2%	-	-
% Total	0.3%	0.8%	0%	1.1%	21.1%	9.1%	0.1%	30.2%	27.7%	40.8%	0.1%	68.6%	-
Lights and Motorcycles	11	28	0	39	769	336	3	1108	1034	1508	4	2546	3693
% Lights and Motorcycles	84.6%	96.6%	0%	92.9%	97.1%	98.8%	100%	97.6%	99.4%	98.4%	100%	98.8%	98.4%
Heavy	2	1	0	3	23	4	0	27	6	25	0	31	61
% Heavy	15.4%	3.4%	0%	7.1%	2.9%	1.2%	0%	2.4%	0.6%	1.6%	0%	1.2%	1.6%

*L: Left, R: Right, T: Thru, U: U-Turn

02_Site Access - TMC

Tue Mar 21, 2023

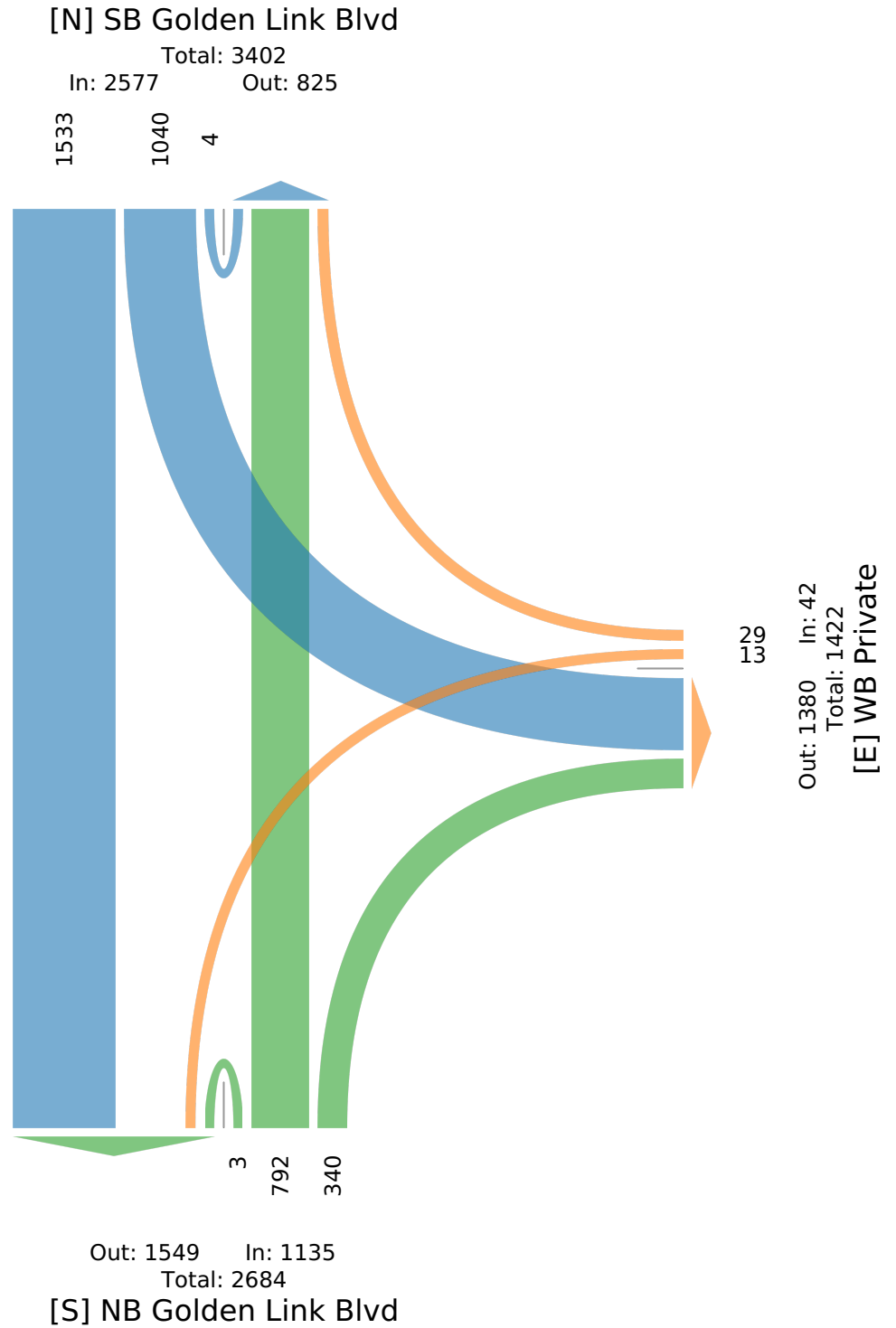
Full Length (1 PM-1 PM (+1))

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US



02_Site Access - TMC

Tue Mar 21, 2023

PM Peak (Mar 21 2023 5PM - 6 PM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				Int
	L	R	U	App	T	R	U	App	L	T	U	App	
2023-03-21 5:00PM	0	0	0	0	19	8	0	27	20	46	0	66	93
5:15PM	0	0	0	0	16	12	0	28	22	42	0	64	92
5:30PM	0	1	0	1	15	5	1	21	21	38	0	59	81
5:45PM	0	1	0	1	19	9	0	28	15	38	0	53	82
Total	0	2	0	2	69	34	1	104	78	164	0	242	348
% Approach	0%	100%	0%	-	66.3%	32.7%	1.0%	-	32.2%	67.8%	0%	-	-
% Total	0%	0.6%	0%	0.6%	19.8%	9.8%	0.3%	29.9%	22.4%	47.1%	0%	69.5%	-
PHF	-	0.500	-	0.500	0.908	0.708	0.250	0.929	0.886	0.891	-	0.917	0.935
Lights and Motorcycles	0	2	0	2	69	33	1	103	78	163	0	241	346
% Lights and Motorcycles	0%	100%	0%	100%	100%	97.1%	100%	99.0%	100%	99.4%	0%	99.6%	99.4%
Heavy	0	0	0	0	0	1	0	1	0	1	0	1	2
% Heavy	0%	0%	0%	0%	0%	2.9%	0%	1.0%	0%	0.6%	0%	0.4%	0.6%

*L: Left, R: Right, T: Thru, U: U-Turn

02_Site Access - TMC

Tue Mar 21, 2023

PM Peak (Mar 21 2023 5PM - 6 PM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

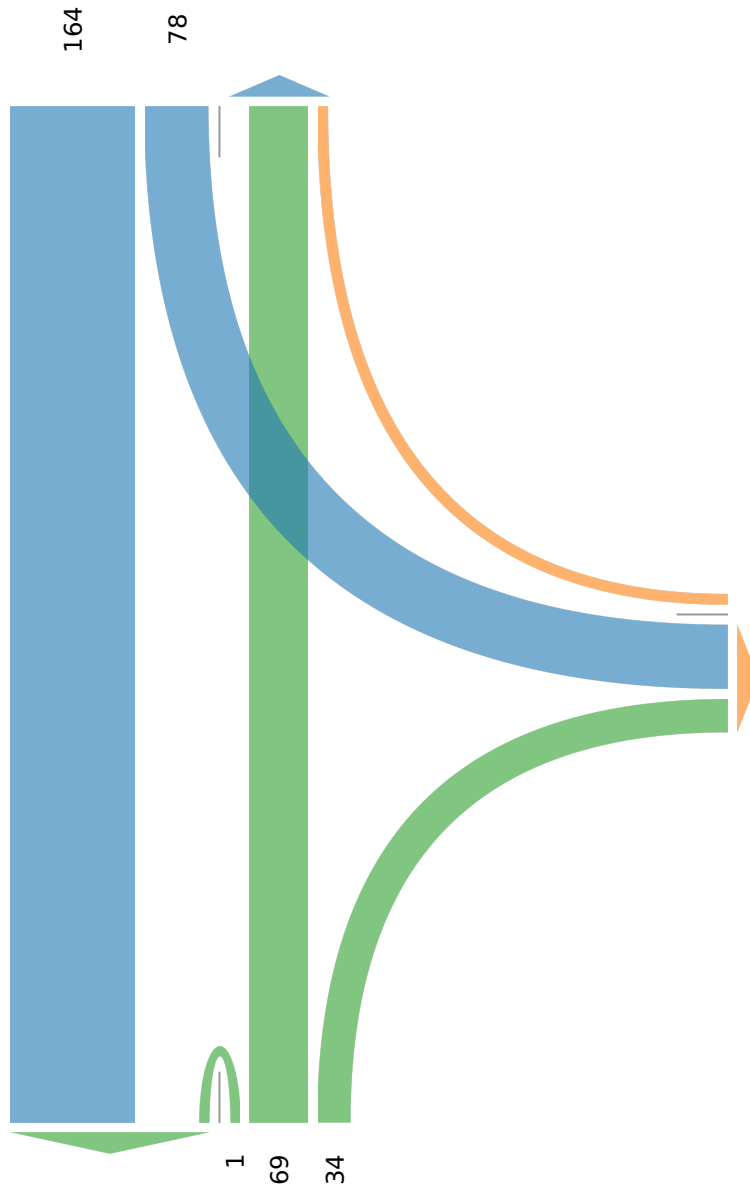
Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US

[N] SB Golden Link Blvd

Total: 313

In: 242

Out: 71



Out: 112 In: 2
Total: 114
[E] WB Private

Out: 165 In: 104
Total: 269
[S] NB Golden Link Blvd

02_Site Access - TMC

Wed Mar 22, 2023

AM Peak (Mar 22 2023 10AM - 11 AM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				
Time	L	R	U	App	T	R	U	App	L	T	U	App	Int
2023-03-22 10:00AM	0	0	0	0	0	1	0	1	6	14	0	20	21
10:15AM	0	0	0	0	3	8	0	11	12	8	0	20	31
10:30AM	0	0	0	0	8	4	0	12	19	14	0	33	45
10:45AM	0	0	0	0	8	4	0	12	28	21	0	49	61
Total	0	0	0	0	19	17	0	36	65	57	0	122	158
% Approach	0%	0%	0%	-	52.8%	47.2%	0%	-	53.3%	46.7%	0%	-	-
% Total	0%	0%	0%	0%	12.0%	10.8%	0%	22.8%	41.1%	36.1%	0%	77.2%	-
PHF	-	-	-	-	0.594	0.531	-	0.750	0.580	0.679	-	0.622	0.648
Lights and Motorcycles	0	0	0	0	19	17	0	36	65	54	0	119	155
% Lights and Motorcycles	0%	0%	0%	-	100%	100%	0%	100%	100%	94.7%	0%	97.5%	98.1%
Heavy	0	0	0	0	0	0	0	0	0	3	0	3	3
% Heavy	0%	0%	0%	-	0%	0%	0%	0%	0%	5.3%	0%	2.5%	1.9%

*L: Left, R: Right, T: Thru, U: U-Turn

02_Site Access - TMC

Wed Mar 22, 2023

AM Peak (Mar 22 2023 10AM - 11 AM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US

[N] SB Golden Link Blvd

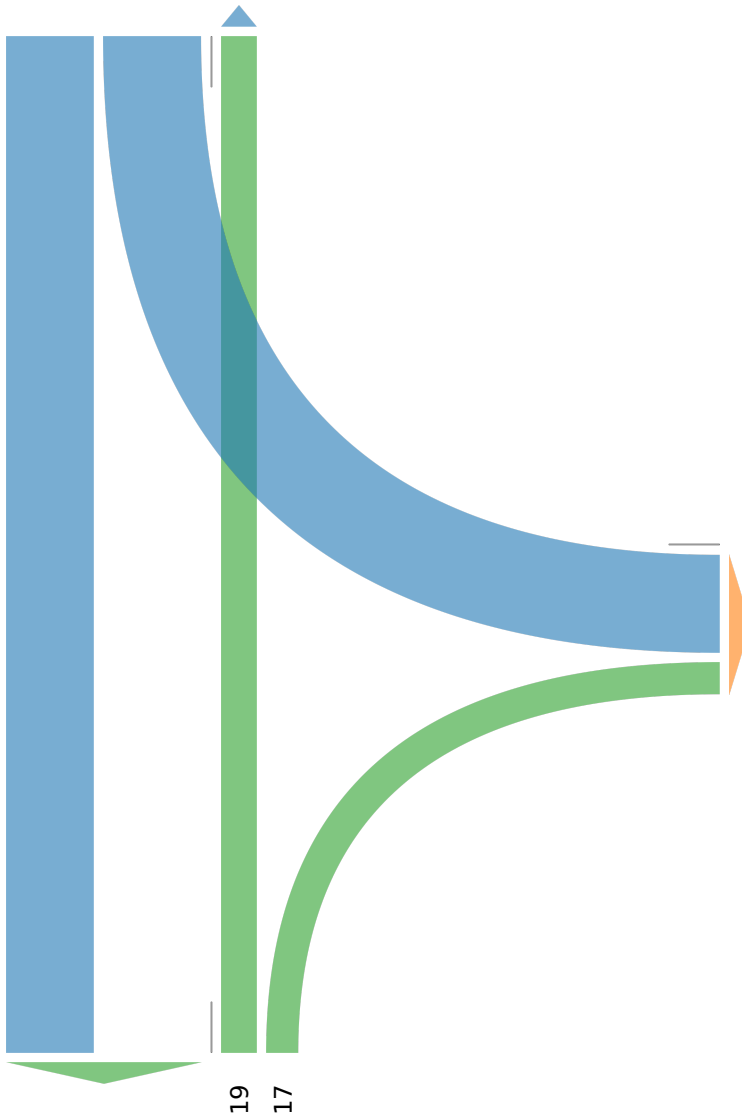
Total: 141

In: 122

Out: 19

57

65



Out: 82 In: 0

Total: 82

[E] WB Private

Out: 57

In: 36

Total: 93

[S] NB Golden Link Blvd

02_Site Access - TMC

Wed Mar 22, 2023

Midday Peak (Mar 22 2023 11:45AM - 12:45 PM) - Overall Peak Hour

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US

Leg Direction	WB Private Westbound				NB Golden Link Blvd Northbound				SB Golden Link Blvd Southbound				Int
	L	R	U	App	T	R	U	App	L	T	U	App	
2023-03-22 11:45AM	0	0	0	0	18	8	0	26	33	53	0	86	112
12:00PM	1	0	0	1	19	14	0	33	53	52	0	105	139
12:15PM	0	0	0	0	36	17	0	53	31	47	1	79	132
12:30PM	1	0	0	1	19	9	0	28	35	48	0	83	112
Total	2	0	0	2	92	48	0	140	152	200	1	353	495
% Approach	100%	0%	0%	-	65.7%	34.3%	0%	-	43.1%	56.7%	0.3%	-	-
% Total	0.4%	0%	0%	0.4%	18.6%	9.7%	0%	28.3%	30.7%	40.4%	0.2%	71.3%	-
PHF	0.500	-	-	0.500	0.639	0.706	-	0.660	0.717	0.943	0.250	0.840	0.890
Lights and Motorcycles	2	0	0	2	88	47	0	135	152	196	1	349	486
% Lights and Motorcycles	100%	0%	0%	100%	95.7%	97.9%	0%	96.4%	100%	98.0%	100%	98.9%	98.2%
Heavy	0	0	0	0	4	1	0	5	0	4	0	4	9
% Heavy	0%	0%	0%	0%	4.3%	2.1%	0%	3.6%	0%	2.0%	0%	1.1%	1.8%

*L: Left, R: Right, T: Thru, U: U-Turn

02_Site Access - TMC

Wed Mar 22, 2023

Midday Peak (Mar 22 2023 11:45AM - 12:45 PM) - Overall Peak Hour

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1047919, Location: 41.312739, -81.523813

Provided by: Kimley-Horn and Associates, Inc.
767 Eustis Street, Suite 100, Saint Paul, MN, 55114, US

